

Main Street Study

Spanish Fork City

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Submitted to

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Executive Summary

Overview

The purpose of this document is to provide an illustrative direction for the community that encourages creative and high-quality urban design along Main Street in Spanish Fork, Utah. This document was authorized by the Spanish Fork City Council and the Utah Department of Transportation. Both entities have various roles and responsibilities in the management of the Spanish Fork Main Street Vision Plan.

Overall this document intends to

- Provide clear objectives for those that plan and design development projects in downtown Spanish Fork and along Main Street from I-15 to Arrowhead Trail,
- Promote a pedestrian and motorist friendly atmosphere,
- Promote original and high-quality urban design of the corridor,
- Enhance the existing character and historical attributes of downtown,
- Develop guidelines for Main Street improvements to be implemented as future projects develop,
- Identify sustainable and timeless design principles,
- Promote a sense of place for the downtown business core and residents,
- Increase the awareness of design considerations among Spanish Fork residents,
- Maintain and enhance property values within downtown Spanish Fork

What the Design Guidelines Address

Zoning codes address the buildings and types of land uses in the downtown corridor that are allowed in particular areas. They also address the intensity of the development. On the other hand, the design guidelines in this document focus on the character and visual elements of the Main Street corridor and the circulation of pedestrians and motorists. The intent of the study and the following guidelines is to create a more business- and pedestrian-friendly downtown that entices business owners to stay and redevelop the downtown area. The guidelines provide design concepts and the tools needed for the stakeholders to implement the vision for the future development of Spanish Fork Main Street.

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PURPOSE



Chapter 1: Purpose of the Spanish Fork Main Street Vision Plan



History

In 2015, Spanish Fork City (City) and local businesses initiated a study to develop revitalization strategies for Spanish Fork Historic Downtown (See Figure 1.1). The original recommendations provided by the American Planning Association included:

- Target, recruit, and maintain a variety of commercial uses in Historic Downtown
- Diversify housing types in Historic Downtown
- Capitalize on key infill development opportunities
- Preserve and enhance buildings in the historic commercial core
- Create design standards and guidelines for development
- Organize promotional events and Merchants Alliance
- Produce a Historic Downtown brochure and walking map
- Establish a Downtown Facilitator' staff position
- Develop, design, and construct a linear park and cultural heritage program
- Redesign and construct a safer Main Street
- Improve parking conditions and
- Implement a sign program for parking, community identity, and wayfinding

Figure 1.1 Main Street is ideally situated as a primary route to several communities in Utah County. Transforming the corridor into a pleasant, pedestrian friendly route will help attract people to the downtown area.



Spanish Fork City has since been implementing many of these recommendations. UDOT and the City initiated the visioning plan project to refine the previous plan for Main Street and develop a concept that can be incorporated with Utah Department of Transportation (UDOT) road projects or built in phases as funding allows.

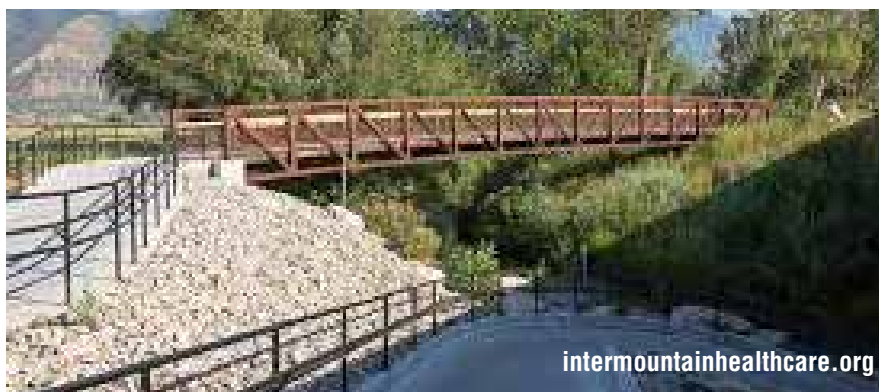
Purpose

The purpose of Spanish Fork Main Street Vision Plan is to create a concept plan with a cohesive design theme that incorporates the following elements:

- a modern boulevard with sustainable landscaping, lighting, and site features
- create design standards and a community theme to establish a sense of place that can be implemented throughout the community
- transportation alternatives that enhance traffic flow and improve safety and
- a project budget with a proposed schedule



Top to Bottom: Figure 1.2 Several historic buildings along Main Street, like the Booth Block Building, add character to downtown. **Figure 1.3** Thousands of spectators line Main Street to watch the Spanish Fork Fiesta Days parade. **Figure 1.4** The Spanish Fork Fair Grounds hosts a number of events year round including Fiesta Days Rodeo, Utah County Fair, concerts, expos and festivals.



Design Theme

To develop a corridor-wide design theme, the design team incorporated four design parameters into the Vision: fiesta, Iceland, Spanish Fork River, and history.

Fiesta: Spanish Fork residents love the annual Fiesta Days. The parade, one of the biggest in Utah, is the heart of the week-long activities in late July. The Main Street Vision takes this event into account and will not impede, hinder, or otherwise impact the parade; rather, the vision will enhance, improve, and celebrate Spanish Fork City (See Figure 1.2).

Iceland: Residents of Spanish Fork identify with Iceland and its heritage. The first permanent Icelandic Settlement in the United States was in Spanish Fork after recently converted Mormon Icelanders were evicted from their country. Immigrants settled here between 1855 and 1860 and a strong heritage still exists. Today this ancestry is celebrated during the annual Icelandic Days in June (See Figure 1.3).

Spanish Fork River: The city is built upon three alluvial fans formed by the Spanish Fork River. Archibald Gardner built the first sawmill in 1858 followed by a flour mill in 1859. The recent development of the River Trail has brought residents back to the rivers edge to appreciate its natural beauty (See Figure 1.3).

History: Spanish Fork High School was built in 1934, the remaining gymnasium was converted into the Nebo School District Offices, and the craftsman bungalow built in 1912 for the Jones family, are icons in this community (See Figure 1.4). These two buildings lie in the south transition zone, a string of historic buildings and facades front the Spanish Fork Main Street downtown zone including Spanish Fork City Hall and the Angelus Theatre. The Main Street Vision incorporates the architecture of these structures into the gateways, the light poles, the art work, and other design elements that will be implemented throughout the corridor.

Top to Bottom: Figure 1.5 The Fiest Days Rodeo attracts local, regional, and national riders for this world class event. **Figure 1.6** Icelandic immigrants settled in Spanish Fork creating the first permanent Icelandic settlement in the United States. **Figure 1.7** The Spanish Fork River Trail attracts community residents to enjoy the outdoors, a pedestrian bridge connects Main Street at Arrowhead Trail.

STREETSCAPE
DESIGN



Chapter 2: Streetscape Design

Main Street Zones

For the Main Street Vision Plan, Main Street was divided into five areas of focus for the purpose of prioritizing the implementation of the plan (See Figure 2.1):

1. Downtown Core Area (Main Street from 400 North to 100 South)
2. Transition Areas
 - North (Main Street from 700 North to 400 North)
 - South (Main Street from 100 South to 300 South)
3. Fairgrounds Area (Main Street from 300 South to Volunteer Drive)
4. Spanish Fork River Area (Main Street from Volunteer Drive to Arrowhead Trail)
5. I-15 Commercial Area (Main Street from the I-15 interchange to 700 North)

The two transition areas and the 100 East area lead motorists into the heart of the City; drivers come north and south from the I-15 exit and from surrounding communities. The transition areas lead motorists into the historic Main Street area. Main Street connects Salem into downtown Spanish Fork coming from the south.

Implementing the Main Street Vision Plan will help the City accomplish its goal of revitalizing the downtown area during the next several years.

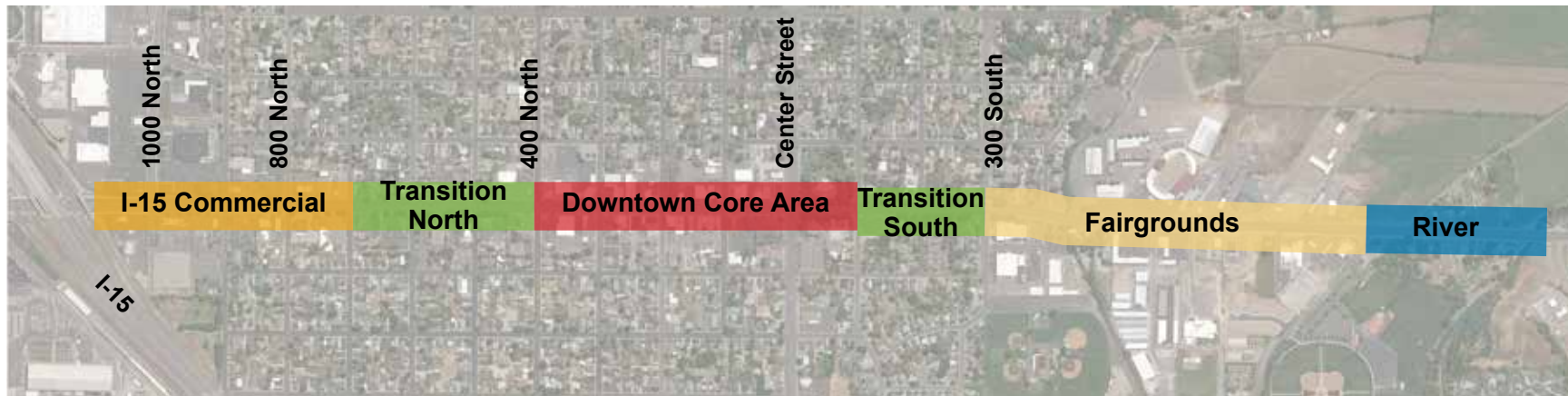


Figure 2.1 Main Street was divided into five segments for the Spanish Fork Main Street Vision Plan. Traffic enters the area along various routes and from various communities surrounding Spanish Fork.



Downtown Core

The five blocks between 400 North and 100 South have historically fronted the majority of businesses on Main Street and were determined to be Spanish Fork's Downtown Core. Most of the design for the Main Street Vision Plan focuses on this area because it was viewed as the greatest catalyst for revitalizing Main Street. In general, the area needs lighting, landscaping, and parking improvements.

Some trees grow in this area, however the tree species provides dense leaf coverage with a tear dropped canopy effectively blocking visibility to store fronts and business identification signage. The current trees planted in 2016 were selected for their root structure to minimize sidewalk heave. The on-street parking is an extension of Main Street traffic lanes with no delineation in paving materials or striping to differentiate between the traffic lanes and the parking. The area allowed for parking is very wide allowing a large buffer zone between traffic and the parking area. There are no traffic calming elements to encourage drivers to decrease speeds creating dangerous situations for pedestrian crossings and vehicular access (See Figure 2.2 to 2.5).



Figure 2.2 to 2.5 Spanish Fork Main Street businesses and the Spanish Fork Chamber of Commerce were influential in development of the Main Street Vision Plan.

Downtown Core Area

The Downtown Core area is located between 100 South and 400 North as established by the American Planning Association in the 2015 Community Planning Assistance Teams (CPAT) assessment to Rediscover Historic Downtown Spanish Fork (See Figure 2.1). The revitalization of the Downtown Core is the primary objective to ensure strong future growth for the community and boost the city's cultural heritage and identity.

In the Downtown Core area, the current car-centric design serves the commuter through-street traffic disregarding area businesses. The wide roadways promote faster driving speeds, unsignalized crosswalks that are unsafe, and limited street lighting making the area feel uninviting. By following the proposed design guidelines, the City can capitalize on the historic presence of the Downtown Core area and create an inviting sense of place. Preserving the existing street wall of buildings while developing rear entrances with shared, easily accessible, and clearly labeled inner block parking will help alleviate the strain of available parking. The streetscape design standards for the Downtown Core include widening the pedestrian zone with creative landscaping, seating, outdoor eating areas, and bulb outs mid-block and at intersections; adding a center landscaped island wide enough for large historic street trees and allowing for a pedestrian refuge at crosswalks; adding a designated bike lane for each direction of through traffic as part of the Spanish Fork Heritage Trail; maintain parallel parking stalls with striping definition; and add pedestrian scale street lighting for safety and area identity (See Figure 2.6).



Figure 2.6 The Downtown Core area covers a five-block segment of Main Street, one of the longest segments in the study.

Road Cross Section: Downtown Core

The existing cross section on Main Street is typically 92 feet wide from curb to curb. The design will modify this cross section to improve and beautify downtown.

Sidewalks

Currently, sidewalk widths vary from 15 to 17 feet. The typical sidewalk width on the west side of the street is approximately 17 feet, and the typical sidewalk width on the east side of the street is 16 feet (See Figure 2.7). In the Main Street Vision Plan, the sidewalks on both sides of Main Street will be 16 feet wide typical and where bump outs occur 25 feet wide.



Figure 2.7 The existing sidewalks on Main Street vary slightly. The Vision Plan proposes to make them the same width and add bump outs for safety and aesthetics.

On-Street Parking

To create a more complete Main Street with space for bike lanes and on-street parking, the width of the sidewalks were balanced in the design to 16 feet on both sides. By adjusting the width of the sidewalks, on-street parking will remain a convenient element of downtown and allow for a dedicated bike lane (See Figure 2.8).

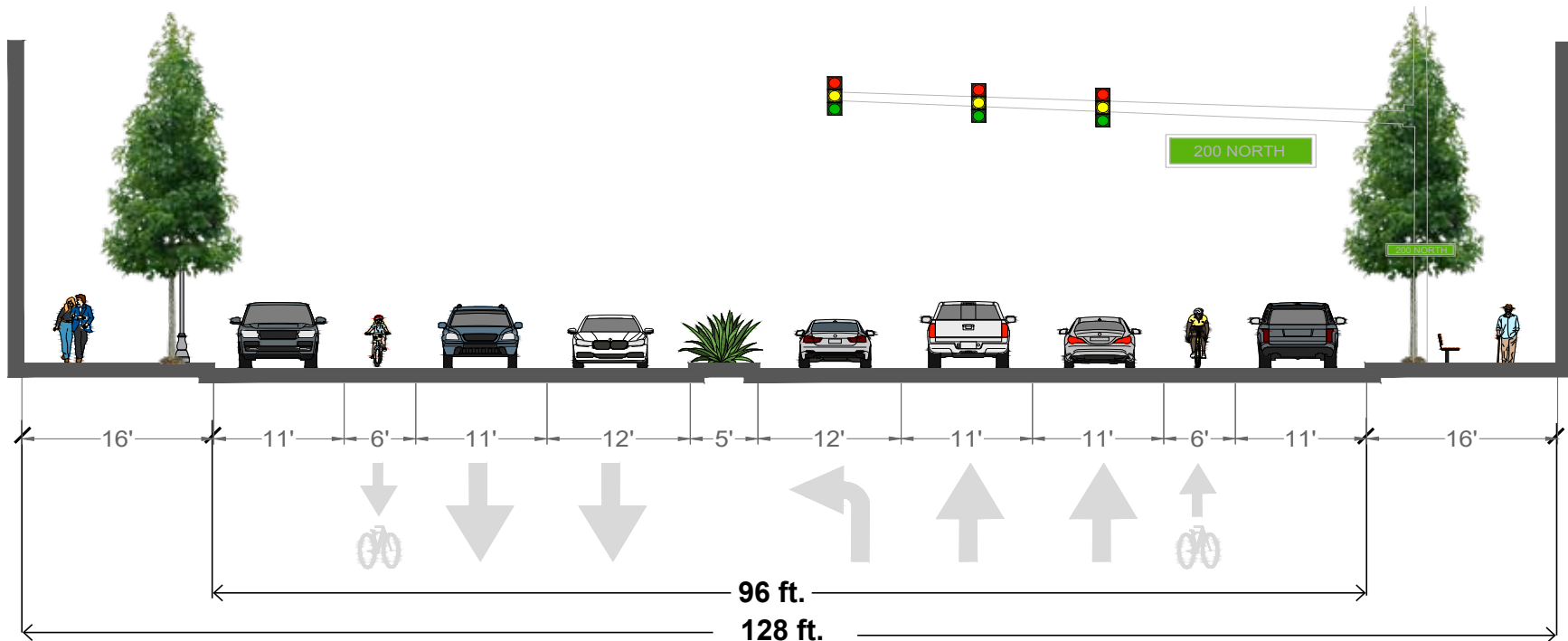


Figure 2.8 The Vision Plan proposes to delineate on-street parking, add bike lanes, and add landscape and hardscape elements to enhance Main Street.

Center Median and Bump Outs

The center landscape median will provide space for small trees, complimenting the sidewalk plantings, which will help create a sense of place without interfering with traffic. The bump outs will provide additional space for outdoor seating, dining, and added landscaping to create a pedestrian friendly environment (See Figure 2.9).

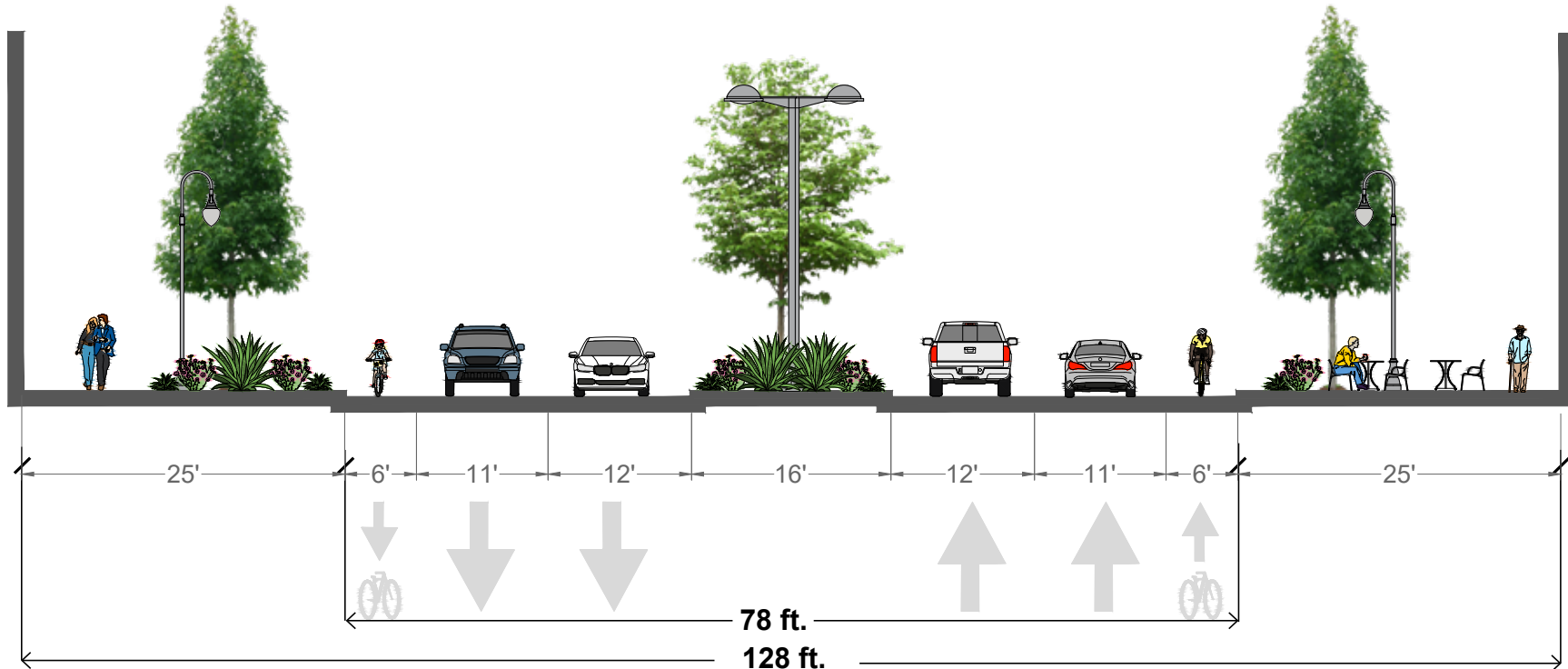


Figure 2.9 The Vision Plan proposes to include a landscaped center median, bike lanes, and landscape bump out elements to enhance Main Street.

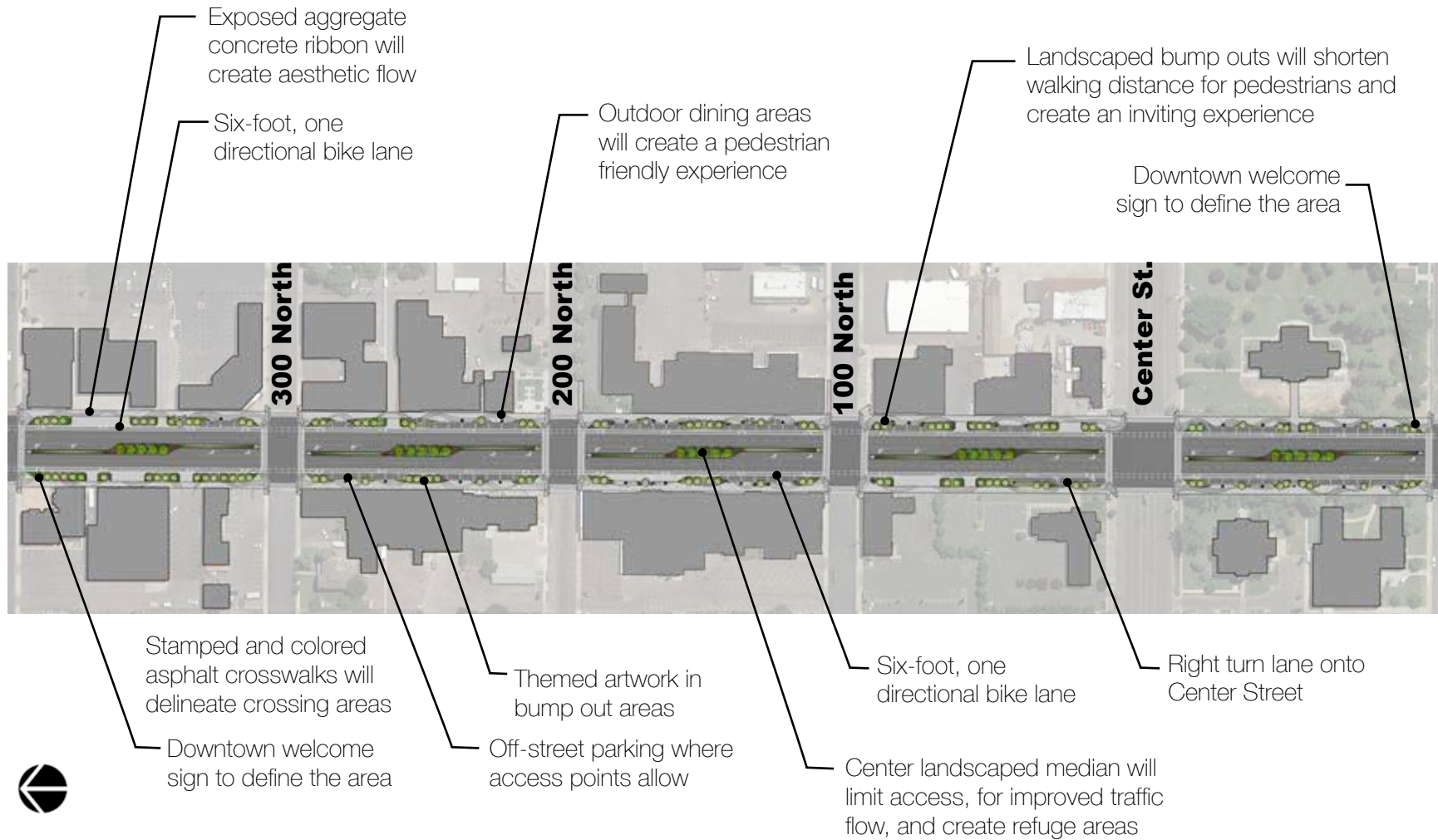


Figure 2.10 The Downtown Core will set the design standard for the Main Street corridor. Lighting, landscaping, hardscape, and other design elements will be reflected throughout the corridor creating a sense of place.



Top to Bottom: Figure 2.11 To create a more visually appealing corridor, a variety of concrete finishes are proposed to represent a flowing river leading the visitor through the space. **Figure 2.12** Stamped asphalt or in-laid reflective tape, such as those pictured above, will help improve the visibility of crosswalks.

Hardscape Elements

Stamped Asphalt and Concrete Finish

Current hardscape materials on Main Street include asphalt and concrete. To create a more visually appealing corridor and differentiate usage, stamped asphalt will be used at intersection crosswalks and a variety of concrete surfaces will be used on the sidewalks (See Figure 2.11 and 2.12). The crosswalk treatments will differentiate the look of the asphalt and create a safer environment for pedestrians. The combination of various hardscape materials within the 10-foot walkway, as well as added landscape bump outs, will create pedestrian interest and safety. In the new sidewalk areas, an exposed aggregate or stamped concrete ribbon will break up the expanse of the concrete. This ribbon will run the length of Main Street through the various areas leading pedestrians through the space and linking the area to the Spanish Fork River.

Due to UDOT maintenance concerns, concrete is not permitted for crosswalks across Main Street. On Main Street, the only allowable surface treatments (such as grooved-in thermoplastic reflective tape or stamped asphalt) may be used inside the crosswalk lines. However, a different hardscape material may be used for the side street crosswalks, because they are within the City's jurisdiction, but for consistency stamped and stained asphalt will be used for all the crosswalks in these areas. The crosswalk lines will most likely be grooved-in thermoplastic markings to prevent tearing by snow plows.



Figure 2.13 Curb cuts at each bump out will allow storm water to flow into the landscape areas.

Curb Bump Outs

At all intersection corners and some mid-block areas, curb bump outs are proposed to encompass the 11-foot widths of the on-street parking areas. These will create space for landscape and also add visual interest to the asphalt. The bump outs will not reduce the width of the travel lanes; instead, the roadway will feel more narrow, which can slow traffic and create more planter space for landscape materials to improve area aesthetics, such as grasses, shrubs, and trees and allow space for seating or outdoor eating areas. These bump out areas on Main Street in the Downtown Core will add a sense of pedestrian safety and reducing the crossing distance at intersections. This will aid in creating a more interesting and visually appealing streetscape, as well as safer on-street parking. In addition, the bump outs will help drivers feel like their vehicles are not obstructing the street (See Figure 2.14).



Figure 2.14 Curb bump outs create space for aesthetic landscape design and traffic buffering.

At each bump out, curb cuts will be made to allow for storm water to flow into the landscape areas, naturally capturing rainwater (See Figure 2.13). The collection of rainwater will augment irrigation water and ensure that the proposed landscape areas are well maintained.

Streetscape Furnishings

Trash Receptacle and Benches

Street furniture, trash receptacles, or benches can be placed on either side of Main Street and in the bump-out areas at the street corners (See Figures 2.15 and 2.16). These furnishings will add to the pedestrian-friendly atmosphere in downtown. All street furniture will need to be built to withstand the harsh Utah climates and be easy to clean, repair, and replace, as necessary. It is recommended that benches be installed in areas with shade for a more comfortable seating experience.



Figure 2.15 Well-made and maintained trash cans will encourage patrons to keep Main Street clean.



Figure 2.16 Benches placed in appropriate locations will help make Main Street more pedestrian-friendly. New benches at Memorial Square on Main Street.

Artwork

Art pieces within the bump-out areas and along the sidewalk will create a new experience for Main Street pedestrians. Bronzed, life-size statues of traditional rodeo and adjacent activities will reflect the deep tradition of community, western heritage, and the beautiful natural environment within and around Spanish Fork City. From the indigenous Native Americans who first settle along the Spanish Fork River to the trek of Spanish missionaries and Icelandic immigrants, and the pioneers that have followed ever since, Spanish Fork continues to grow upon a strong foundation of explorers and visionaries. These art pieces will remind local residents and guests of the community developed by those pioneers and natural environment that defines this town in many ways (See Figures 2.17, 2.18, and 2.19). Since statues will not be moveable, the pieces should be placed far enough from the road to not create a conflict with snow removal.



Figures 2.17, 2.18 and 2.19 Bronze art pieces such as these pictured will add a unique element to Main Street. Art pieces are proposed to be placed at key locations in the downtown area.



Figures 2.20 These solar powered transit shelters are lit for safety and optional glass surround offers weather protection while allowing full visibility. They will create places for pedestrians to sit and wait for transit services.

Transit Shelters

With the construction of the new Center Street Intermodal Center, the Front Runner Commuter Rail Line will increase ridership for transit services in Spanish Fork. Local bus routes will need to be enhanced and provide additional coverage and stops along Main Street.

The Intermodal Center is scheduled to be part of Phase 2 and targeted for completion between 2025-2034, however the Bus Rapid Transit and Enhanced Bus Service is not planned until Phase 3 or after. To create a comprehensive vision for the future of Main Street areas for transit, shelters need to be included to accommodate this eventual amenity (See Figure 2.20)

Lighting

Lighting will enhance the look and feel of Main Street (See Figure 2.21). In the five block area of 400 North to 100 South, two types of street lighting are proposed:

- 24-foot, colonial style, powder-coated light poles with a pendant light are planned to be placed at all intersection corners (pending UDOT approval); and
- a smaller 18-foot acorn style light should be spaced every 50 feet between intersections, between the taller light poles.

The varying heights of the lamps will create a vibrant look and feel along Main Street. The light poles can be built to hold banners for upcoming City events or for seasonal decorations. Hanging flower pots can also be hung from the lamp posts to add color and texture beyond the planters placed at ground level.



Figure 2.21 Smaller street lamps are proposed for the downtown core closer to the buildings. Taller lamps are proposed for the intersections within the downtown core and each intersection and mid-block for the other Main Street zones. Installing decorative street lamps, like the ones pictured here, will provide a pleasant feature to Main Street and flexibility to promote seasons or City events.



Figure 2.22 The shared light pole is another option to light the Main Street corridor.

Shared Light Pole

A lighting option where two pendant lights share the same pole was also explored as a viable lighting solution along the Main Street downtown core area (See Figure 2.22). These types of poles have been used in other city streetscapes.

By using two pendant lights on one pole and reducing the number of vertical poles used overall, the cost of lighting could be reduced. However, implementing this concept would mean less variety in lamp styles, since the same light would be used on each arm.

Signage

Creating an entrance sign at the beginning and end of the Downtown Core area will help to distinguish the area and further enhance a sense of place. The signs should match or compliment the Spanish Fork River Trail signage using corten steel and stainless steel for a contemporary rustic look (See Figures 2.23 and 2.24). Wayfinding signs also need to be added indicating points of interest, activity centers, and public parking areas (See Figure 2.25).



Figures 2.23 and 2.24 Examples of corten and stainless steel entrance signs. **Figure 2.25** Example of wayfinding sign in Logan, Utah.

Transition Areas

The transition areas are located on the north and south ends of the Main Street Downtown Area (See Figure 2.1). The improvements to the transition areas are secondary to the improvements in the downtown area.

In the transition areas, the majority of the parkstrip areas have inconsistent landscaping, with turf in some areas. These areas should be consistent. By following design guidelines, the City can make the landscape of the transition areas consistent with the landscape in the downtown core area. Additional trees will need to be added in the transition areas to separate the sidewalk from the road. Street lights will need to be added to the area, with each light spaced 60 to 65 feet from the next. The street lights will be 24-foot tall and match those placed in the downtown core area. Flower pots as well as banners can be added to the light poles for additional visual interest (See Figure 2.26 to 2.29).



Figure 2.26 Each transition area covers a two to three-block segment of Main Street where features lead into and out of the Downtown Core.

Road Cross Section: Transition Areas

The roadway cross section in Figure 2.27 shows the improved lighting that will match the Downtown Core. These lights will be spaced slightly further apart, with 75 to 100 feet between each light.

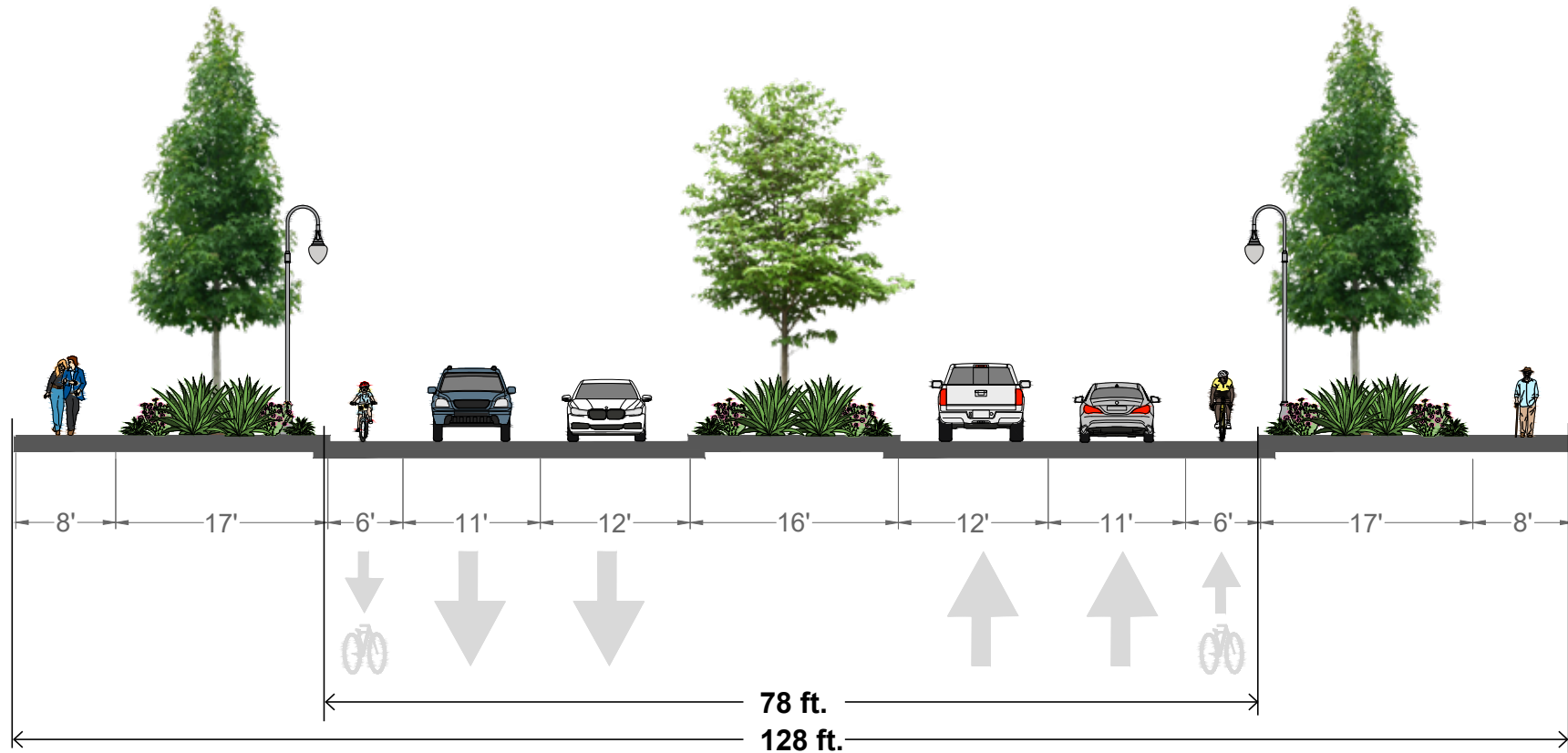


Figure 2.27 In the transition areas, the roadway lane configurations and widths will remain the same as they are today. Landscape and street lamps will be added. Existing large shade trees will remain where space and sight lines allow.



Figure 2.28 The North Transition Area will continue the center median from the Downtown Core, however, the decreased sidewalk width and building setbacks create wider landscape planter beds that can accommodate the existing London Plane trees, if sight lines allow.



Figure 2.29 The South Transition Area will continue the center median from the Downtown Core, however, the decreased sidewalk width and building setbacks create wider landscape planter beds that can accommodate the existing London Plane trees, if sight lines allow.

Interstate 15 Commercial Area

The gateway area of the Interstate 15 Commercial Area is located at the north end of the Main Street Study Area (See Figure 2.1). The improvements to the commercial area are secondary to the improvements in the downtown area.

In the I-15 Commercial Area, the streetscape design and sidewalk layout is inconsistent. The proposed design guidelines for this area will help make this area consistent and relate with the landscape of the rest of Main Street. The existing sidewalk location varies and is nonexistent in certain sections. Establishing and maintaining a consistent design throughout this section is important for continuity. On the west side of the road between 900 North and 1000 North, the sidewalk needs to be completed. Additional trees will need to be added to establish a continuous sight line down the street. Street lights will need to be added to the area and match those placed in the Downtown Core area. Flower pots as well as banners can be added to the light poles for additional visual interest. The existing annual flower beds should be expanded to match the Downtown Core area and be planted with perennial flowers and grasses to decrease maintenance. To provide a gateway entrance into Spanish Fork, a center median to match the Downtown Core is recommended between 900 North and 1000 North as well as from 1000 North to the I-15 interchange access ramps and restripe lanes to add six-foot bike lanes to both directions of traffic. After the completion of the Center Street Intermodal Hub, an extended transit route will be implemented increasing bus routes along Main Street to 1000 North turning east to US Highway 6, additional bus stops will be required to better serve this commercial area (See Figures 2.30 to 2.32).



Figure 2.30 The I-15 Commercial Area covers a four-block segment of Main Street where features lead into and out of the Interstate 15 interchange.

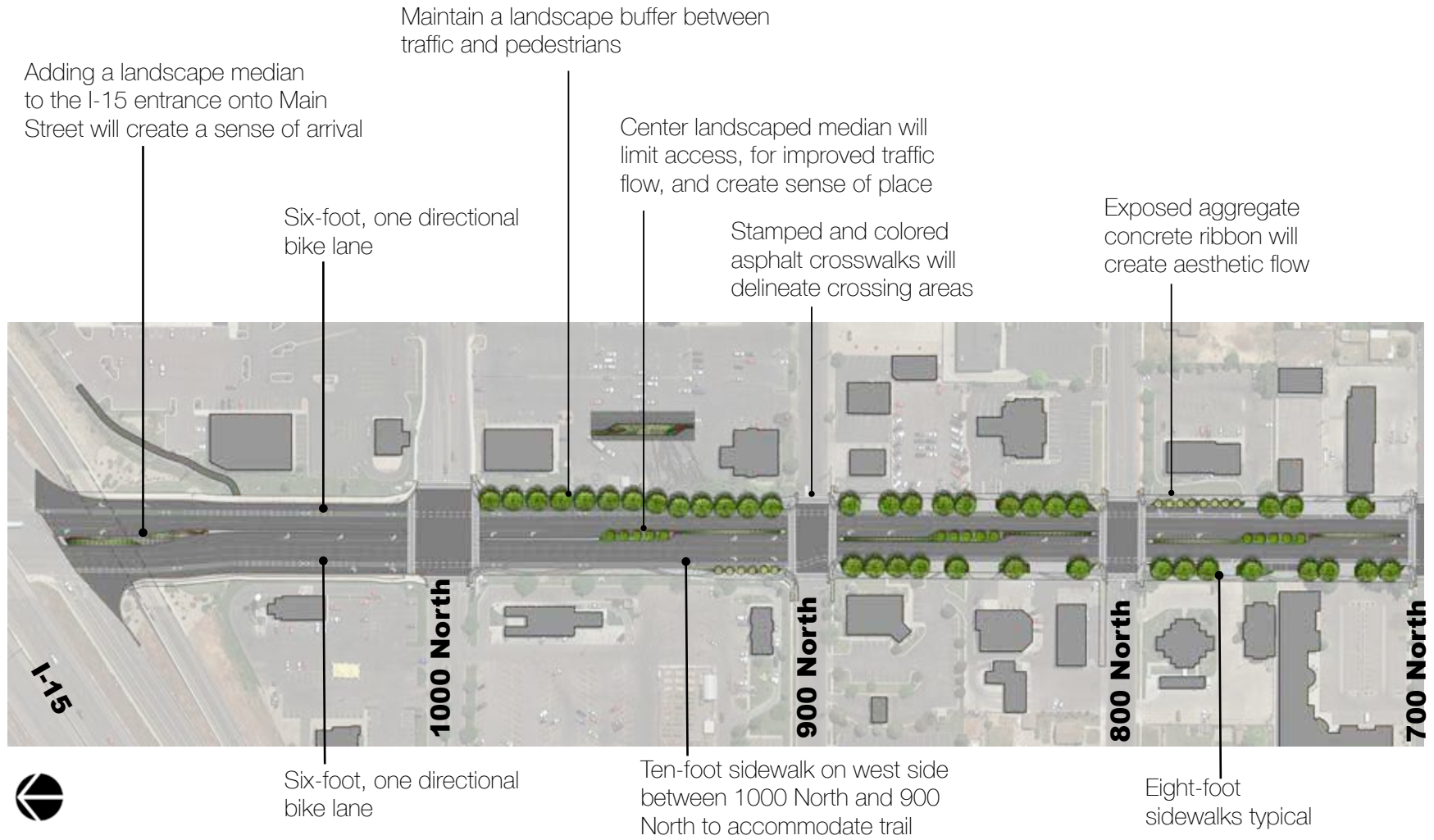


Figure 2.31 Incorporating the features proposed in the Vision for the I-15 Commercial Area into UDOT's design plans will help save costs and accelerate when these elements are built.

Road Cross Section: I-15 Commerical Area

The roadway cross section in Figure 2.32 shows the continuation of landscape elements that will match the Downtown Core. These lights will be spaced to match the Transition Areas. The sidewalk width and location varies on the blocks closest to Interstate 15.

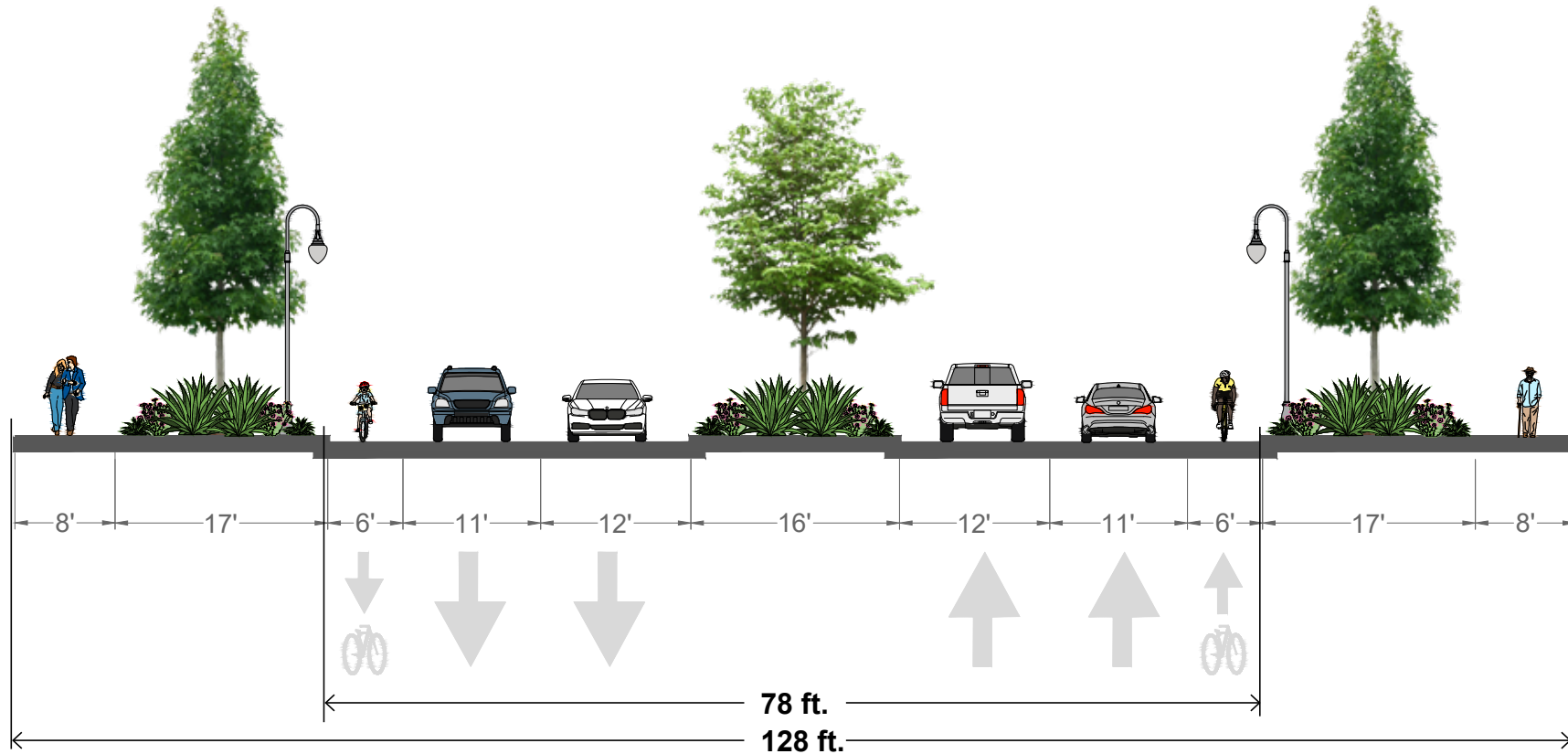


Figure 2.32 In the I-15 Commercial Area, the roadway lane configurations and widths are adjusted slightly to accommodate the bike lanes. Landscape and street lamps will be added.

Fairgrounds Area

The gateway of the Fairgrounds Area is located at the south end of the Main Street Study Area (See Figure 2.1). The improvements to this area are tertiary to the improvements in the downtown area.

In the Fairgrounds Area, the majority of the parkstrip areas have inconsistent landscaping, with turf in some areas. These areas should be consistent. The pride of the Spanish Fork residents are the established London Plane Trees that sporadically line the street between 100 South and 400 South, however this species requires a larger planting area to accommodate its trunk flare and root capacity as well as a greater distance from overhead utilities based on crown size. The design guidelines for this area reflect the need to allow for the existing trees. By following design guidelines, the City can make the landscape of this area consistent with the landscape of the rest of Main Street. Landscape and sidewalk have yet to be added from 660 South to Volunteer Drive on the east side. Additional trees will need to be added to create the visual continuity through this zone. Street lights will need to be added to the area to match those placed in the Downtown Core area. Flower pots as well as banners can be added to the light poles for additional visual interest. As traffic increases in this area, access will need to be monitored and possibly restricted to allow ease of traffic flow (See Figure 2.33 to 2.35).



Figure 2.33 The Fairgrounds Area covers an approximate four-block segment of Main Street where features are influenced by the Fairgrounds and its events.

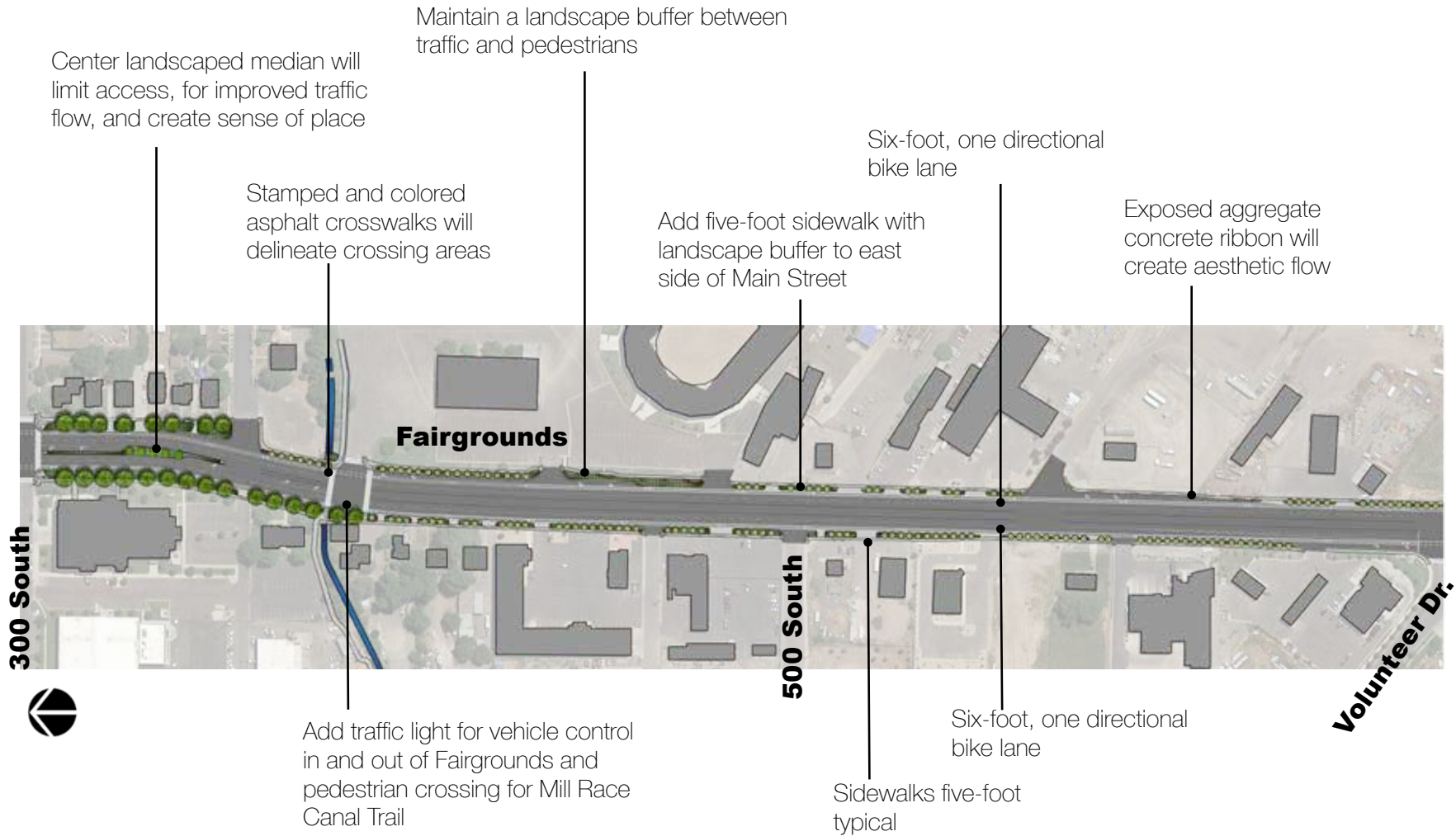


Figure 2.34 Incorporating some of the features from the Downtown Core such as the paving ribbon and tree choices help to extend Main Street's sense of place.

Road Cross Section: Fairgrounds and River Areas

The roadway cross section in Figure 2.35 shows the improved lighting and continuation of landscape elements that will match the Downtown Core. These lights will be spaced slightly further apart, with 150 feet between each light.

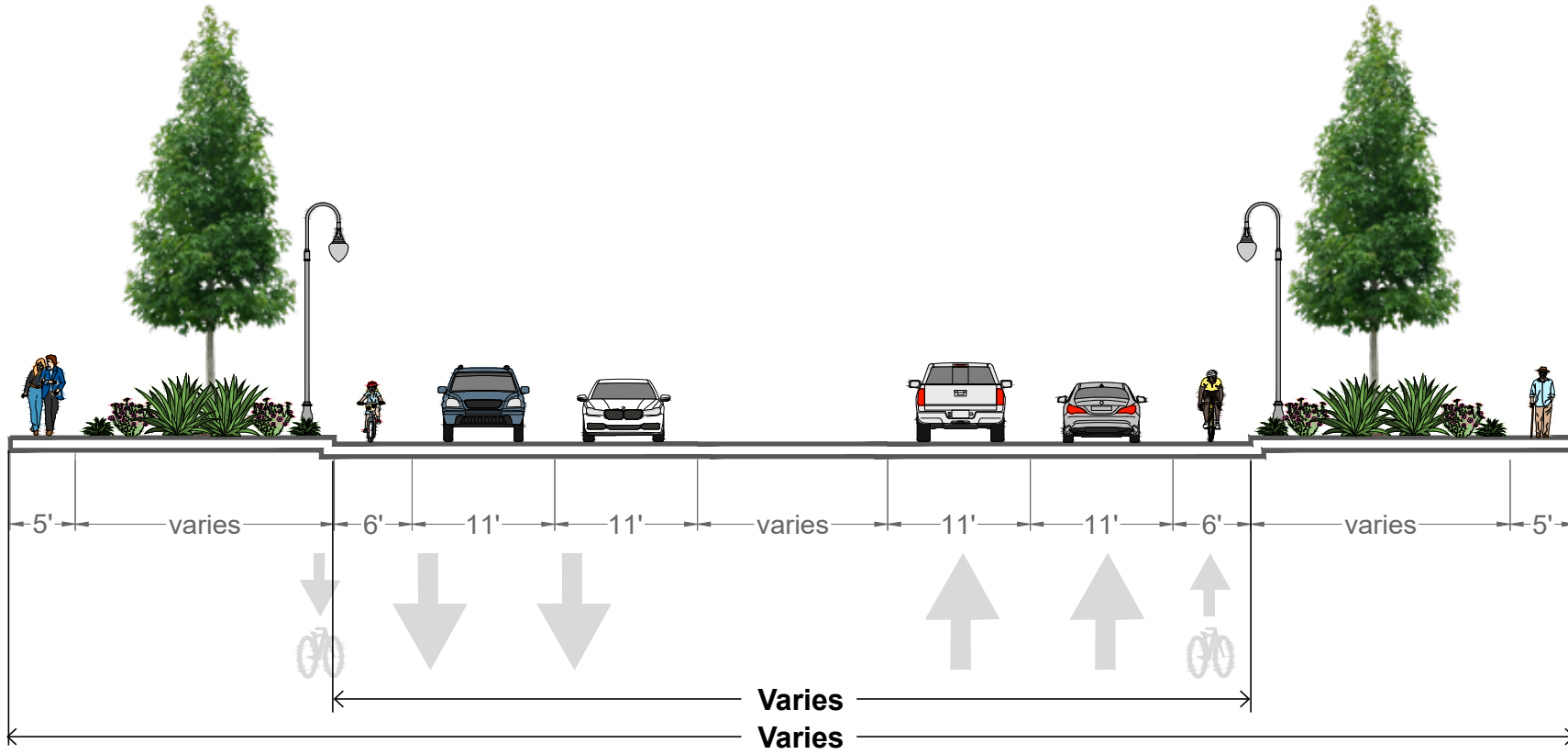


Figure 2.35 In the Fairgrounds and River Areas, the roadway lane configurations and widths will remain the same as they are today. Landscape and street lamps will be added. Existing large shade trees will remain where space and sight lines allow.

River Area

The transition area of the River Area is located at the southmost end of the Main Street Study Area (See Figure 2.1). The improvements to this transition area are tertiary to the improvements in the downtown area.

In the River Area, sidewalk and landscape improvements have only been added to the west side of the street. There is pedestrian access across the river on the east side where it connects to the river trail. In order to connect the river trail to the commercial areas of Spanish Fork, sidewalk and landscape improvements to the east side of the street are recommended. These improvements should be consistent with the Main Street Fairgrounds Area and Downtown Core. By following design guidelines, the City can make the landscape of this more rural area consistent with the landscape of the rest of Main Street. Additional trees will need to be added to create a consistent visual appearance. Street lights will need to be added to the area to match those placed in the downtown core area. Banners can be added to the light poles for additional visual interest and advertising for civic events (See Figure 2.36 and 2.37).

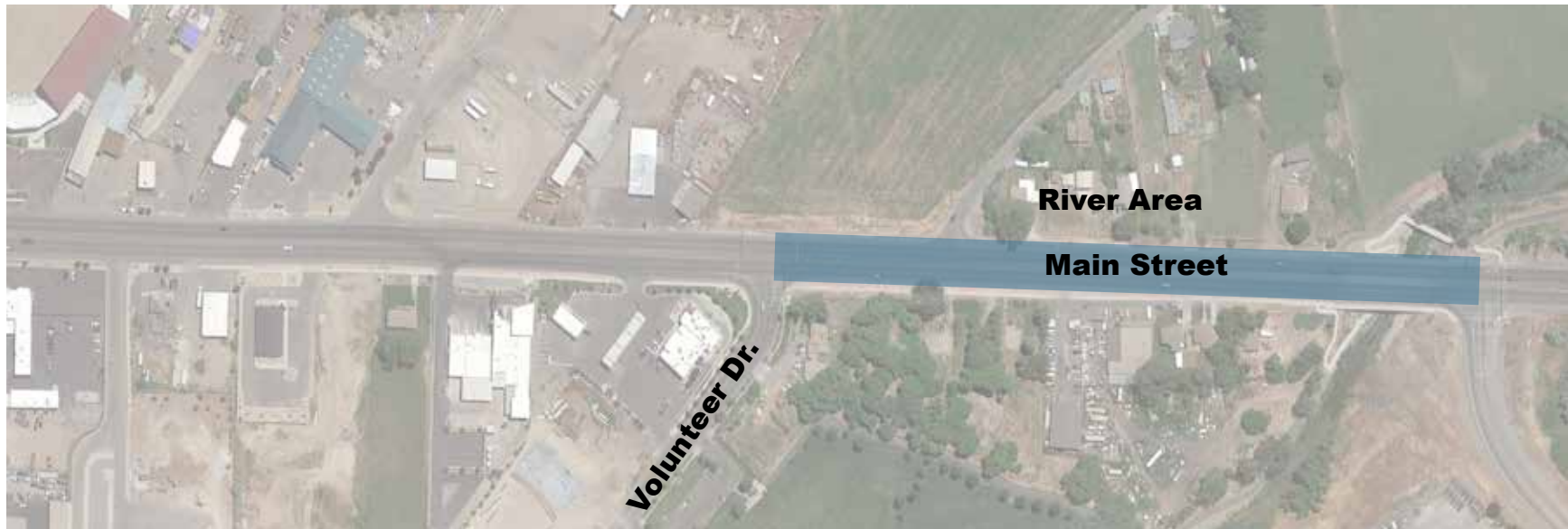


Figure 2.36 The River Area covers a rural segment of Main Street where features start the transition between urban and rural areas.

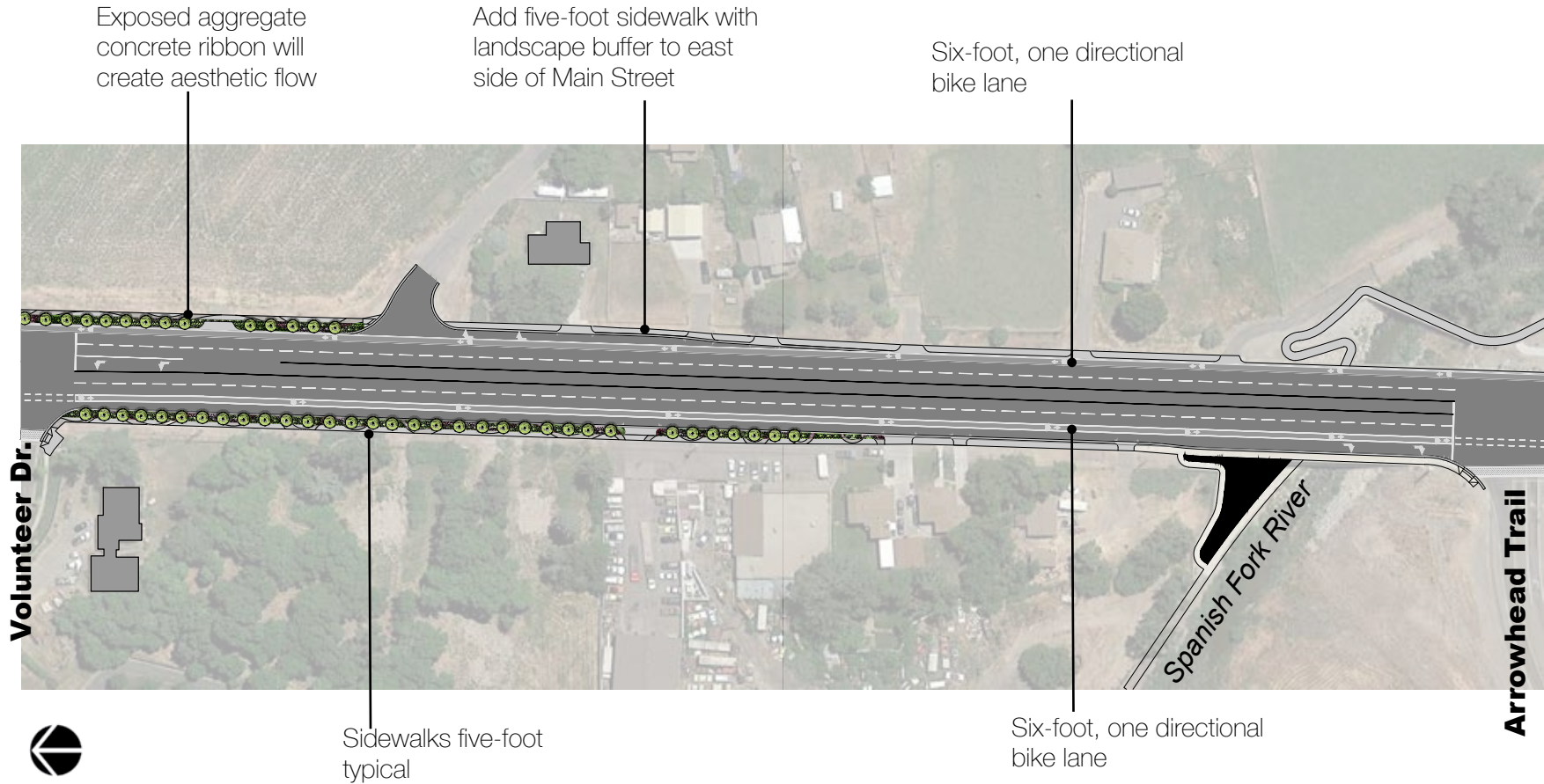


Figure 2.37 Continuing some of the features from the Downtown Core such as the paving ribbon and tree choices help to extend Main Streets sense of place and tie it to the Spanish Fork River.

Design Guidelines

A streetscape that is built properly, with quality materials, and has a consistent appearance is necessary to creating a pedestrian-friendly environment that invites people into downtown Spanish Fork. If the aesthetic treatments are built in phases, adhering to the guidelines listed below will be vital to ensure that the site features are compatible with the surrounding hardscape elements and theme.

Maintaining consistent hardscape features in the downtown core area will contribute to the distinctive character desired for Main Street. A combination of pavers, lighting, and planter boxes will add variety and texture to the street and enhance the public's experience and encourage them to spend time downtown. These features must be built out of durable material to withstand Utah's harsh climate. As such, Spanish Fork will need to ensure repairs to hardscape elements due to vehicle crashes or vandalism are paid for (in-part or wholly) by the associated insurance organizations (See Table 2.1).

Structure Type	Guidelines	Maintenance/Life Expectancy
Sidewalks	<ul style="list-style-type: none"> Sidewalks will be ten feet wide minimum in the downtown core and 8 feet wide in transition zones and I-15 Commercial Zone, the Fairgrounds and River Zones are 5 feet wide A ribbon of exposed aggregate concrete stained or treated with Natina depicting the flow of the Spanish Fork River will carry the pedestrian through the area (See Appendix A) Sidewalk subgrades should be compacted to reduce settlement 	<ul style="list-style-type: none"> Sidewalk should be cleaned and sealed regularly Stain and concrete should be checked annually for fading and cracks New stain or concrete treatment will need to be reapplied every two to three years
Street Lights	<ul style="list-style-type: none"> Street lights will be such that banners and hanging pots can be attached to them Street lighting along the downtown core will consist of two types of street lamps: <ul style="list-style-type: none"> 24-foot poles with pendant style lamps 18-foot poles with acorn style lamps 	<ul style="list-style-type: none"> Banners and hanging pots should be inspected regularly to ensure they are fastened securely to lamp poles Light bulbs will need to be replaced promptly Lamp poles should be inspected regularly for vandalism and repaired as needed
Planter Beds	<ul style="list-style-type: none"> Landscaping will consist of two types of mulch: a large 4"-6" cobble to carry the river band from the sidewalk design into the landscape and a smaller 1" cobble or dark brown bark mulch for the remainder of the planter bed area Planter beds will be drip irrigated and planted with perennial drought tolerant plant species (See Appendix A) 	<ul style="list-style-type: none"> A one to two-inch layer of bark mulch should be replenished yearly after spring clean up Cobble will need to be cleaned up regularly and replenished every three to four years

Table 2.1 Streetscape Design Guidelines

Structure Type	Guidelines	Maintenance/Life Expectancy
Curbing	<ul style="list-style-type: none"> All curb bump-outs must have curb cuts to allow for storm water to be harvested into landscape areas at the corners of the off-street parking Curbing must be a minimum of 6 inches in height 	<ul style="list-style-type: none"> At least two curb cuts must be made along each bump-out to ensure storm water is harvested into landscape areas
Hanging Pots	<ul style="list-style-type: none"> Planters should be easy to remove for maintenance and during the winter Hanging pots will be a durable lightweight material filled with light weight potting mix with water storing crystals Pots will be drip irrigated to maintain a consistent watering schedule 	<ul style="list-style-type: none"> Containers should be removed and stored for the winter to ensure a longer life span Hanging pots will be replanted with annual flowers each spring and rehung
Crosswalks	<ul style="list-style-type: none"> Main Street crosswalks should be differentiated from asphalt paving with colored and stamped asphalt or grooved-in thermoplastic tape A 12-inch wide, white band of paint should border both sides of the paver treatment at all crosswalks to further contrast the roadway asphalt and the crosswalk 	<ul style="list-style-type: none"> Painted white bands should be inspected regularly to ensure they maintain a distinct separation between asphalt and crosswalk
Artwork	<ul style="list-style-type: none"> Art pieces will reflect the community traditions of Spanish Fork City Art pieces will be displayed in sitting areas, specifically at intersection corners in curb bump-outs areas where there is ample room and in the center median All art pieces must be anchored securely into a pedestal or directly into the concrete 	<ul style="list-style-type: none"> Art pieces should be regularly inspected for vandalism and repaired immediately
On-Street Parking	<ul style="list-style-type: none"> On-street parking areas will be 9 feet wide and 22 feet in length 	<ul style="list-style-type: none"> Life span of pavers is 15 to 20 years Pavers in on-street parking areas should be inspected every 5 to 7 years and repaired as needed
Transition Zones	<ul style="list-style-type: none"> Landscape treatments that reflect the proposed downtown core landscape will be incorporated into the existing park strip To create a more uniform and consistent canopy between the sidewalk and the road, additional street trees will be added Street lighting in all transition zones will match the 24-foot colonial light poles in the downtown core area 	<ul style="list-style-type: none"> For transition zone maintenance, refer to landscape and street light sections
Structure Type	Guidelines	Maintenance/Life Expectancy

Table 2.1 Streetscape Design Guidelines



LANDSCAPE DESIGN

Chapter 3: Landscape Design

Landscape Features

Several years ago, trees were planted along the Main Street sidewalks. Over the years, these trees struggled and slowly grew creating small dense canopies effectively blocking business storefronts and signage. These trees are in narrow, oval landscaped islands along the Main Street park strip and provide little shade or aesthetic softening of the urban landscape.

The new downtown will include the addition of larger landscaped bump outs and landscaped center medians, trees, and hanging flowering pots. Incorporating drip irrigation into all planters, hanging

pots, and other landscaped areas will ensure that all new landscape will remain healthy and irrigation water will be used efficiently. Storm water retention in the planter beds will help naturally irrigate landscaped areas.

The planter areas and landscape beds will be positioned at the intersections and mid-block so that pedestrians can feel somewhat isolated from ongoing traffic. Grasses, shrubs, and other colorful perennials will provide added interest to the streetscape as well as complement the new hardscape paver treatment (See Figure 3.1).

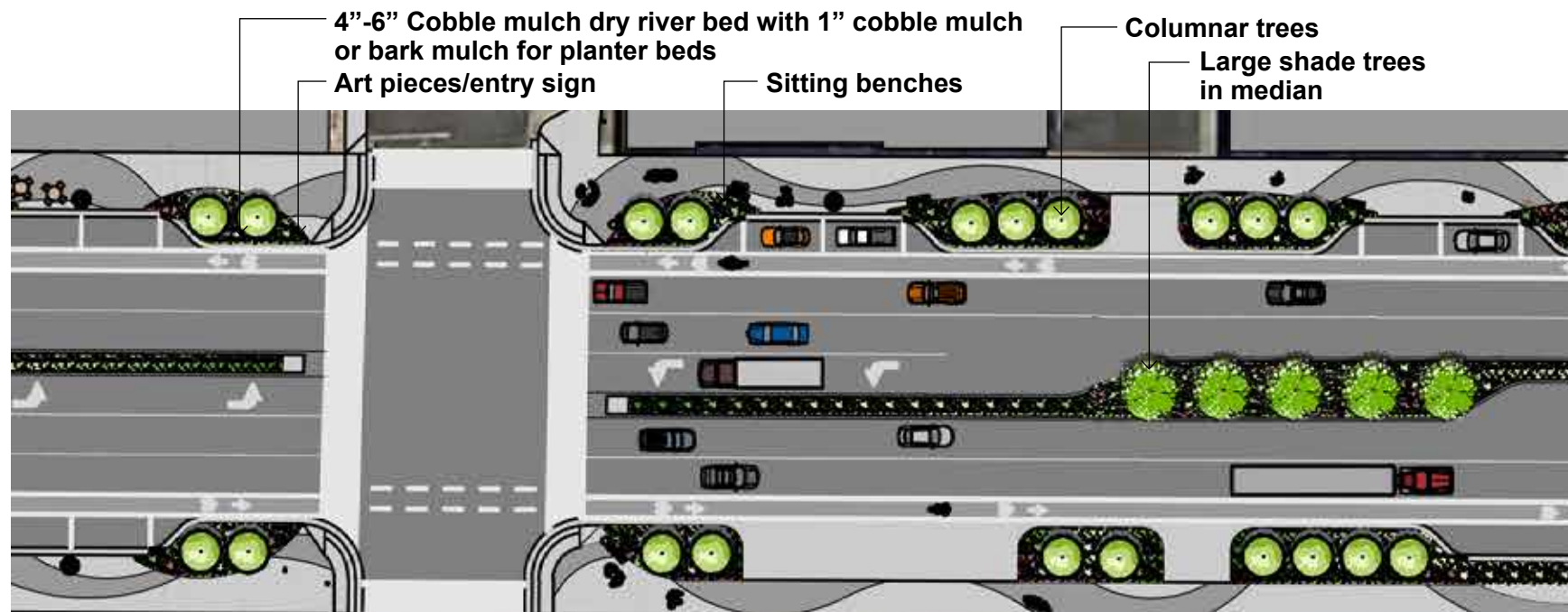


Figure 3.1 Adding landscape features in the curb bump outs will shorten the distance for pedestrians to cross the streets and increase area aesthetics.



Trees

Adding the right combinations of plants and trees to the sidewalk area will provide texture and color to the corridor throughout the year. Trees that are columnar in shape will best suit the narrow streetscape on the east and west sides (See Figures 3.2 to 3.4).

These types of trees will provide some shade and allow pedestrians to pass by without having to duck under branches. Trees will be spaced so that businesses will remain visible from the road. The trees will also help to serve as a buffer from the busy roadway.

To minimize root damage to sidewalks and pavement, mechanisms to force deep root growth will need to be used.

The center median provides an area for another tree species, creating visual variety. To create continuity throughout the corridor another columnar tree can be used. (See Figure 3.5).

Figure 3.2 to 3.4 Using a visually appealing columnar tree close to the buildings, like a Capital Flowering Pear, to provide shade and create a buffer from the street. **Figure 3.5** Continuing down the central median with trees of a similar shape and size, like a Sweetgum, can be done to create a distinct sense of place.



Landscape Bump Outs

Taking back some of the asphalt at intersections and adding landscape within extended curb bump outs will add dramatically to the overall streetscape aesthetic. The bump outs will help conceal on-street parking from within the roadway and shorten the distance pedestrians have to travel to cross the street effectively creating a safer, more pedestrian friendly Main Street.

Although the overall roadway profile has not been compromised, these landscape bump outs will serve to calm traffic throughout the downtown core area. The harsh transition between sidewalk and roadway that currently exists along Main Street will be softened with the use of landscape (See Figures 3.6 and 3.8).

Hanging Pots

Flowering pots hung from street poles will bring another dimension to the streetscape (See Figure 3.7). Hanging pots have been used in Utah cities throughout the state.

Clockwise from Left to Right: Figure 3.6 By taking some of the asphalt to create landscape bump outs it decreases the distance for pedestrians to cross at intersections. **Figure 3.7** Hanging pots can be used to further enhance the color of Main Street from spring through fall. **Figure 3.8** Using a combination of shrubs, grasses, and perennials, will add color and variety to the curb bump outs.

Design Guidelines

Landscape plants and trees soften the feel of the streetscape and add texture and color for much of the year. They also help create a pedestrian-friendly environment for the Downtown Core. Where possible, plants should be native and perennial. Trees, which are proposed to line Main Street, should grow well in alkaline soils and

withstand the salt and brine solutions applied to Main Street during the winter months. Using drip irrigation in the planter boxes and the hanging planters will help keep plants healthy and vibrant while reducing the need to water the plants by hand (See Table 4.1).

Feature	Guidelines	Maintenance/Life Expectancy
Trees	<ul style="list-style-type: none"> • Columnar oaks and Sweetgum trees are recommended for downtown • The species should be consistent throughout the Downtown Core or at least patterned between two species • Trees will be concentrated mostly at the intersections with a few trees placed along mid-block stretches to not block store fronts • Before trees are planted, they will need to be staked with two to three 1.5-inch wide, 8-foot long plastic, cloth, or rubber stakes that are driven into the hole on the side from which the prevailing winds come • Trees should be planted at least 20 feet from utility poles • Trees should be planted at least 3 feet from any hardscape or curbing to ensure adequate space for root growth and possible trunk flare • Depending on the type of native soils found prior to planting, soil amendments may need to be added to ensure proper drainage and conditions that will allow for the best growth of the trees 	<ul style="list-style-type: none"> • Although columnar trees have been selected, pruning should be performed regularly to ensure that branches do not inhibit pedestrians from using the sidewalks • Trees should be inspected yearly for any damage caused by snow removal, disease, lack of water, or any other blight that may prohibit them from growing in natural form and design • Plant materials such as leaves and twigs that drop due to seasonal changes or severe wind and weather should be cleaned up occasionally to maintain a clean streetscape
Grasses/ Annuals/ Perennials	<ul style="list-style-type: none"> • Ornamental grasses and perennials are recommended in planter areas to provide drought tolerant plantings that can survive the snow load that piles up on roadsides in the winter • Plant spacing and selection should be such that bed areas look full and have a variety of color and texture • Annuals will be used in hanging pots only 	<ul style="list-style-type: none"> • Cool season grasses, such as feather reed grass, should be cut back to 6 inches in late winter or early spring • Planter areas should be weeded as necessary to keep plants healthy • Plants should be fertilized as necessary • Flowers in hanging pots will need to be checked regularly to maintain moisture

Table 3.1 Landscape Design

Feature	Guidelines	Maintenance
Irrigation	<ul style="list-style-type: none"> • Irrigation systems must be in place prior to vegetation installation to ensure that plants maintain their natural form and appearance • Drip irrigation with a three- to four-inch mulch cover will provide the most efficient water source to maintain healthy plant materials • Drip irrigation lines will be run to hanging pots for consistent and thorough watering, provide two to three emitters per pot based on size 	<ul style="list-style-type: none"> • Irrigation drip lines and emitters should be checked yearly to prevent obstructions and ensure proper water flow to plants • Irrigation systems should be cleaned yearly by opening flushing valves in the main line, sub-main lines, or laterals while under pressure to push out any pollutants and prevent lines from freezing during the winter months

Table 3.1 Landscape Design

4

PUBLIC OUTREACH

Chapter 4: Public Outreach



Figure 4.1 City staff, business owners, UDOT, and community members met in advisory committee meetings to review and develop a unified vision for Main Street.

Advisory Team Meetings

Overview

Project Engineering Consultants (PEC), the Utah Department of Transportation (UDOT), and Spanish Fork City (City) met in late April to kick off the Main Street Vision Plan project. The group discussed the project scope and proposed a pedestrian friendly, transit-oriented Main Street with more trees, landscaping, and medians.

During the kick-off meeting for the Main Street Vision Plan, City staff expressed their desire to develop a vision for Main Street that created a sense of place, where aesthetic treatments and walkability were balanced with moving traffic and where businesses could thrive. Staff also wanted to broaden the public involvement effort for the new vision study.

A 13 person advisory committee, including members from the Spanish Fork Chamber of Commerce, key business owners and stakeholders, the City staff and council, UTA, and UDOT, was created to ensure these elements were incorporated into the Vision (See Figure 4.1). The advisory committee met every three to four weeks from May through October with the goal of working through PEC's conceptual vision and coming to a consensus of the elements to present to the mayor and City council.



Figure 4.2 and 4.3 Comment post-its from participants at the public open house.

Meetings

April 30, 2018 Meeting

The group reviewed the project limits and discussed if the Downtown Core area identified in the American Planning Association (APA) study should be expanded. They determined that future trail connections to the north and south of the study area should be considered. The new study area would be extended to I-15 at the north and as far south as the bridge over the Spanish Fork River. In addition to extending the project limits, the team wanted to make sure the vision for Main Street would be compatible with the projected traffic data. The team determined to stay with the projected 2040 volumes from Utah's Unified Transportation Plan and when the 2050 data becomes available, the vision could be updated to reflect those numbers.

The City wanted to make sure parking (e.g. connectivity and on-street) and wayfinding were top priorities as part of the Main Street vision. One of the problems they are experiencing is employees park on the street in front of the businesses, which reduces the amount of available parking for customers. Another parking challenge would be connecting to lots behind the businesses. Some owners do not want drivers crossing properties and the vision would need to reflect the current situation while proposing options for both interconnectivity and accommodating mass transit. Project aesthetics (e.g. bulb-outs, medians, etc.) should improve safety and make the area more pedestrian friendly and create a “complete streets” feel. The group wanted the vision to reflect a balance of commuter traffic through the area and people traveling to destinations within the Main Street area.

Other items discussed were the location of the future UTA FrontRunner station and the proposed I-15 interchange at Center Street. These changes could have significant impacts to current traffic and use patterns on Main Street. The group recognized these changes would need to be evaluated further once UDOT, UTA, and MAG establish a more defined time frame and at that time, make amendments to the vision plan.

As part of the public outreach, PEC also drafted a survey—with input from the project group—to be distributed to businesses and residents before the June 2018 public open house.



Figure 4.4 Scroll plot of Main Street at public open house.

June 5, 2018 Meeting

This meeting focused primarily on reviewing the draft public survey and preparing for the June 21 public open house. Several survey questions were revised especially the question about what design features (roadway improvements and amenities) are most important for the vision to focus on. For the public open house, PEC drafted a needs matrix identifying key features for the vision and to help meeting attendees comment the proposed design features. In addition to the matrix, the team would provide post-it notes for the public to write their comments on the note pad and place the comment next to a corresponding image found on the project boards.

The group wanted to add current level-of-service, traffic volume data and trail nodes to the base map to provide the public with as much information as possible and help them understand the full scope and vision for Main Street.

Design Charette, August 1, 2018

A design charette was held on August 1, 2018 following the June 21 public open house. The committee discussed different alternatives that were prepared.

- Example Layout 1 - 7 Lane Vehicle Priority Concept
- Example Layout 2 - 5 Lane Multi-Directional Bike Lane Concept
- Example Layout 3 - 5 Lane Center Median Landscape with Bike Lanes
- Example Layout 4 - 5 Lane Partial Center Median with Separated Bike Lanes

Adding an additional lane of traffic to both sides was eliminated, but the addition of bike travel lanes and their location was discussed. The consensus was to include a one directional bike travel lane to each side of the roadway between traffic and off-street parking. A center median was reviewed and the idea of creating a refuge area for pedestrians crossing the street was popular, so the inclusion of a center median was approved as shown in Example Layout 3 (See Figures 4.5 to 4.8).

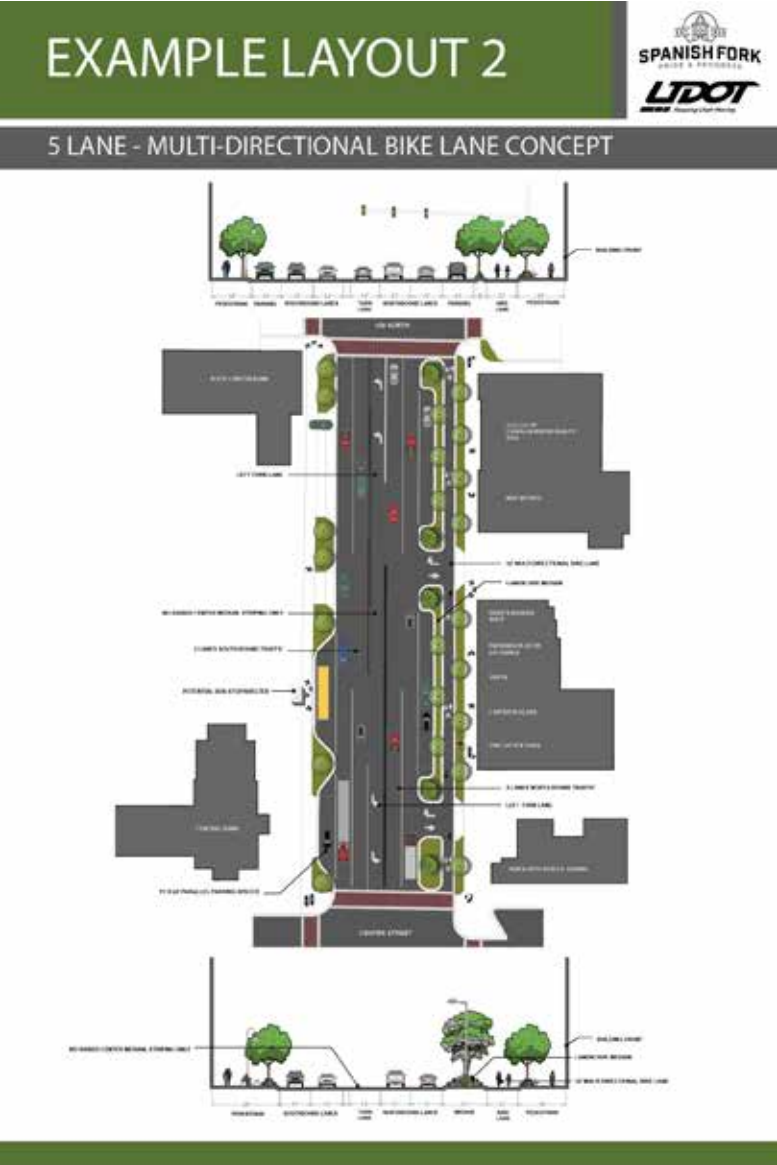
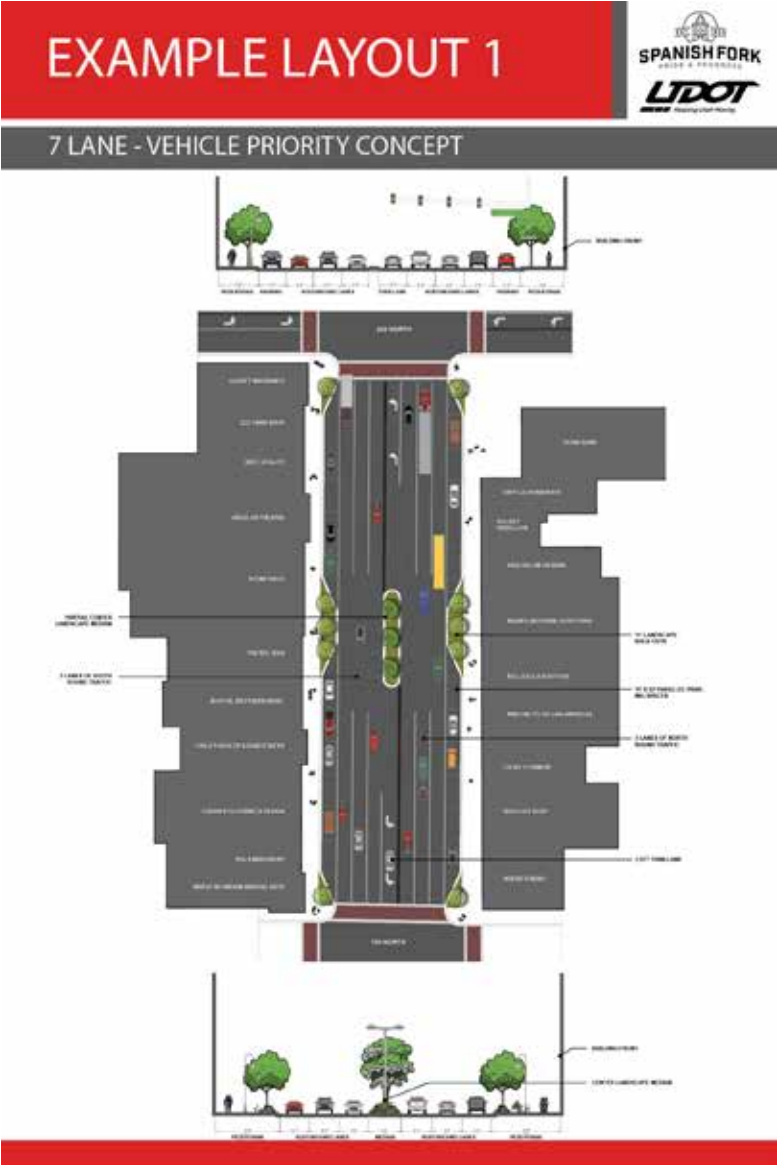


Figure 4.5 Example Layout 1 board for the design charette showing a seven-lane-vehicle priority concept. Figure 4.6 Example Layout 2 board for the design charette showing five-lane multi-directional bike lane concept.

EXAMPLE LAYOUT 3

5 LANE - FULL CENTER MEDIAN LANDSCAPE WITH BIKE LANES



EXAMPLE LAYOUT 4

5 LANE - PARTIAL CENTER MEDIAN WITH SEPARATED BIKE LANES

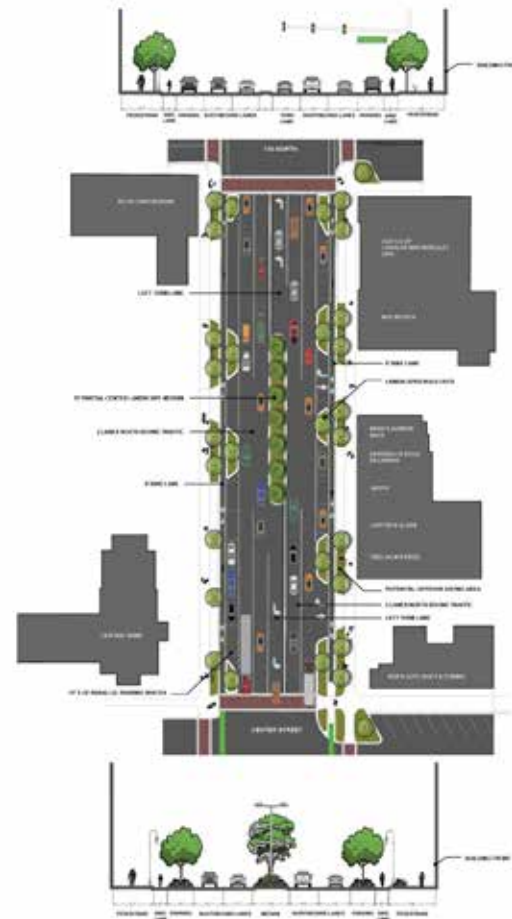


Figure 4.7 Example Layout 3 board for the design charrette showing a five-lane-full center median landscape with bike lanes concept. Figure 4.8 Example Layout 4 board for the design charrette showing five-lane partial center median with separated bike lanes concept.

Concept Presentation, October 22, 2018

On October 22, 2018, PEC presented the concepts developed in concert with the advisory committee to a joint session of the Spanish Fork City Council and Planning Commission. Council members embraced several concepts with some mixed responses regarding the shared lane markings on Main Street. No decisions were made at this meeting. All meeting summaries and attendance rosters are available in Appendix C: Public Involvement Materials.

Public Open House


To increase stakeholder engagement in the development of the Main Street Vision, the project team held one public open house on June 21, 2018 (See Figure 4.9 and 4.10).

The project team developed a four-pronged communication strategy providing various opportunities for residents and business owners in Spanish Fork to review the materials and comment on the proposed vision (See Figures 4.2, 4.3, and 4.11 to 4.16).

PUBLIC OPEN HOUSE

Spanish Fork Main Street Study

Study considering options to improve functionality of Main Street



Spanish Fork City, in consultation with the Utah Department of Transportation, is considering options to improve safety, traffic flow, transit usage, pedestrian access, and aesthetics of Main Street between 300 North and Arrowhead Trail. You are cordially invited to attend a public open house on the following date:

Date: Thursday, June 21, 2018
Time: 6:30-8:00 p.m.
Where: Spanish Fork City Council Chambers, 80 South Main Street Spanish Fork, Utah 84606

In 2015, the city, Metropolitan Association of Governments (MAG), and representatives from the American Planning Association (APA) conducted a planning study to identify opportunities to revitalize the downtown Main Street Area. The Community Planning Assistance Team (CPAT) Report recommended 12 strategies to improve the downtown core area of Main Street. The current study is evaluating four of the CPAT study recommendations.

The meeting will be conducted in an open house format. No formal presentation is planned. However, project staff will be available to gather feedback and answer questions. An online survey is also available for Spanish Fork residents and businesses to participate in. Visit www.sffcity.com/transportation. The survey will close on **July 15, 2018**.

If you are unable to attend the meeting, the information will be posted on the Spanish Fork City website: sffcity.org or you can contact the project team at 877-495-4246. If you have any comments or concerns, we would like to hear from you.





Figure 4.9 In addition to sending residents information about the vision plan in the City's monthly newsletter, a flier was created and delivered to the business owners within the study area. Multiple communication tactics were used to inform and involve business owners and residents. **Figure 4.10** More than 25 people attended the open house to review the materials and give feedback to the project team.

WELCOME



SPANISH FORK MAIN STREET VISION



Figure 4.11 Welcome board for public open house displaying cultural icons of the community.

LANDSCAPE IDEAS



STREET CORNER LANDSCAPING



STREET MEDIAN LANDSCAPING



Figure 4.12 Landscape ideas board for the public open house showing possible landscaping for the street corners and the street center median.

STREETSCAPE IDEAS



SIDEWALK STREETSCAPE



OUTDOOR DINING STREETSCAPE



Figure 4.13 Streetscape ideas board for the public open house with options for sidewalk and outdoor dining streetscapes.

AMENITIES



SITE FURNITURE



SCULPTURES



Figure 4.14 Amenities board for the public open house displaying ideas for site furniture and sculptures.

LIGHTING & PAVING



STREET LIGHTING



STREET PAVING



Figure 4.15 Lighting and paving board for the public open house showing possible street lighting and street paving options.

STREET SECTIONS



STREET SECTION EXAMPLES



PEDESTRIAN CROSSINGS AND WALKWAYS

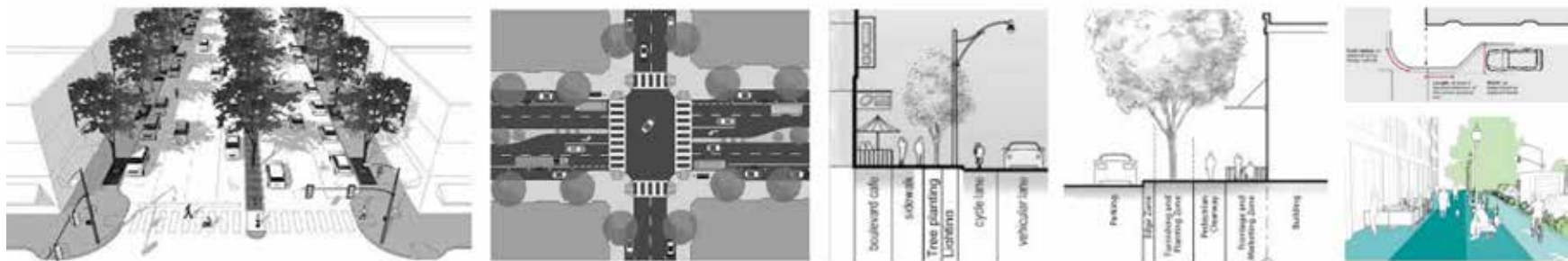


Figure 4.16 Street sections board for the public open house showing examples of street sections, pedestrian crossings, and walkways.

Spanish Fork Main Street Vision

Survey



Q1: Do you currently live or work in Spanish Fork (circle one)?

Yes

No

Q2: According to the image below, in which zone of Spanish Fork do you live/work (circle one)?

- Zone 1
- Zone 2
- Zone 3
- Zone 4
- Zone 5



Q3: I define the downtown Spanish Fork as (circle one):

- The Main Street area between 500 North and 300 South
- The business area along 1000 N between Main Street and U.S. 6
- The area between the U.S. 6/I-15 interchange and Expressway Lane
- Other location (please list)

Public Survey

Working with Spanish Fork Community Development, PEC developed multi-question survey to gather information on how residents use Main Street, what the destination areas are, what the priorities are for amenities in the Main Street area, and their opinions regarding the parking situation. The survey also asked Main Street business owners to identify their challenges in operating a business and what improvements they would like to see. After the project team approved the survey, it was uploaded to Survey Monkey and a link created, which the Spanish Fork Chamber of Commerce and the city disseminated to the community (See Figure 4.17).

More than 640 responded to the survey with over 35 percent of respondents coming from the area between Main Street and I-15 followed by those who reside between Main Street/Canyon Road and U.S. 6, and the southeast quadrant by the mouth of Spanish Fork Canyon. The majority of respondents defined Main Street between 500 North and 300 South as the downtown area of the city. This is important because there is a large commercial center just on 1000 North and at U.S. 6 so key services and businesses should remain in that area. Respondents said the main reasons they go to Main Street is for dining, shopping, and to conduct business or personal errands. It is likely with the new interchange planned at Center Street and I-15, that Main Street will need to further evolve and become the hub to conduct business.

Respondents said they prefer to have more parking behind the Main Street businesses followed by angled parking or more parallel parking on the road. Providing angled parking on Main Street would require UDOT approval, however, if more parking is available behind the buildings, it would most likely free-up spaces on Main Street that are currently being used by employees. The business owners/operators also identified parking as one of the greatest challenges they experience currently.

Figure 4.17 First page of public survey.

Respondents were split on how walkable and pedestrian friendly Main Street is today. To improve the walkability of Main Street, respondents indicated that adding more pedestrian crossings and wider sidewalks were the most important improvements to make on Main Street. Wayfinding signs to parking areas, area restaurants, and parks were also identified as somewhat to very important. Installing these types of signs can help improve the walkability of the corridor without much modification to the existing area.

Transit is not used much by Spanish Fork residents, however the survey indicated that providing connections to the future transit stops (e.g. FrontRunner) is important. The city will need to continue to work closely with UTA to provide connections as the southern part of the Utah Valley grows. For additional information about the survey and results, please see Appendix C: Public Involvement Materials.

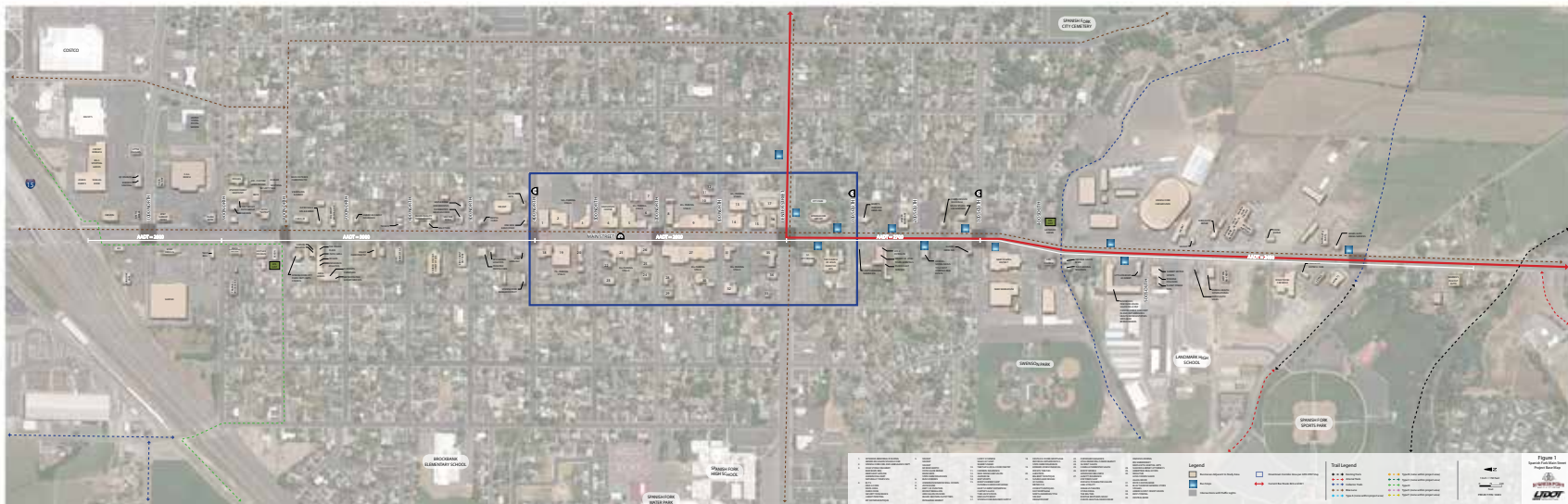
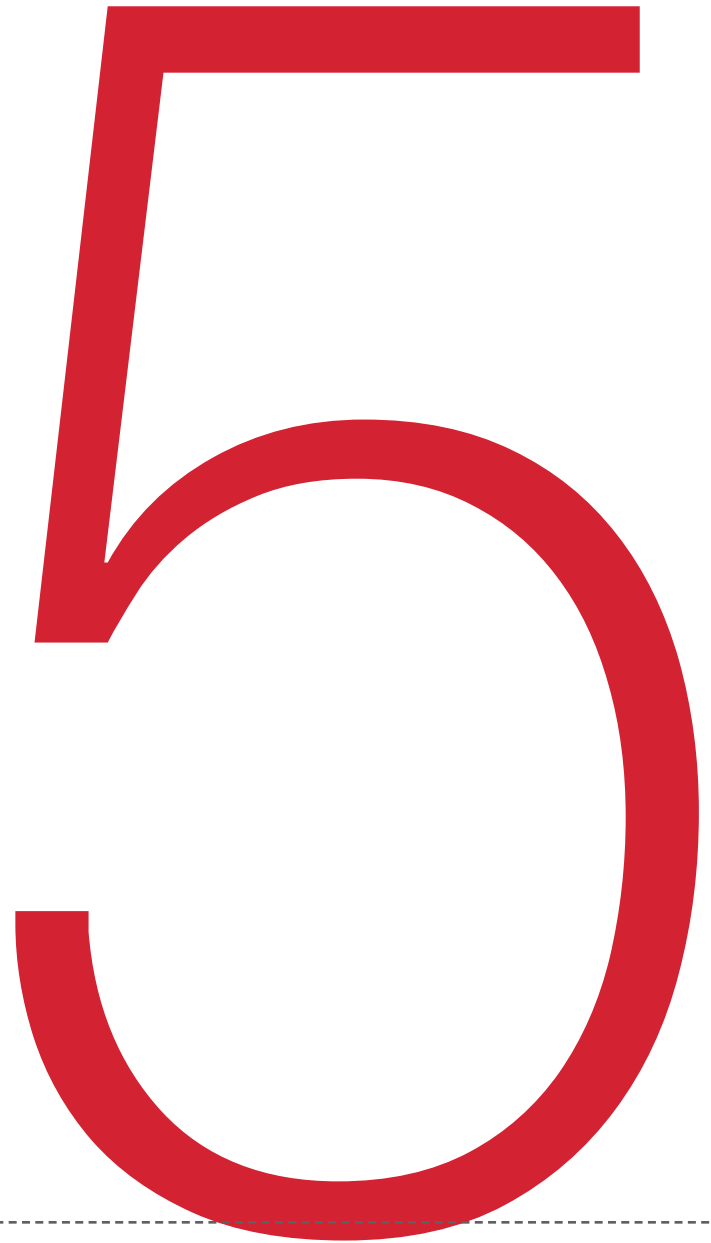


Figure 4.18 Scroll plot of Spanish Fork Main Street project area.

Number of Comments	Topic	Comment Descriptions
4	Transit	Not a large need for transit services at this time, however an increase in bus service may be needed once the Center Street Front Runner station is operational. Not fans of dedicated bus lanes, but extending the bus route could bring more people to downtown. Improving or adding bus stops with benches and shelters.
12	Pedestrian/ Bike Lanes	Participants want to keep the wide sidewalks or make them wider and were divided as to whether a bike lane needs to be added. There were many comments concerning pedestrian safety and improving crosswalks including bump outs, medians, and pedestrian visibility.
8	Lighting	More lighting for sidewalks is needed; Direct light down; Less light pollution; and, like the options to hang banners and flower pots.
11	Traffic	Slowing down traffic on main street but maintaining traffic flow with two lanes each direction was a priority and received many comments.
12	Parking	Don't like losing parking on Main Street, more parking was the number one comment; and, will need rear entrances to businesses if parking is behind buildings. Enforcement of parking restrictions a must.
8	Landscape	Adding landscaping to create a more inviting downtown and keeping the existing trees in front of City Hall and at the City Park, but business owners felt the trees lining downtown blocked store fronts limiting visibility to patrons. Use waterwise planting to conserve water. Hanging baskets of flowers add a nice touch.
9	Site Furniture	More trash receptacles and benches were requested. Maintain them once installed. Install a trash receptacle on every block. Add benches where there is shade, could get sponsors to help with cost.
5	Theme	Respect and preserve our history. Things to remind us of our heritage like Escalante and the farmer in Canyon Creek. Possibly a painted mural with the history of Spanish Fork.
8	Aesthetics	Create a sense of place. Incorporate outdoor dining and maybe add speakers for music or events. Add wayfinding signs.
A total of 15 people submitted comments at the October 21, 2015 open house.		

Table 4.1 Summary of comments submitted at the open house.

COST &
IMPLEMENTATION PLAN



Chapter 5: Cost and Implementation Plan

Overall Cost

The estimated total cost to build all of the designed improvements along the Main Street corridor, as outlined in the Main Street Vision Plan, is \$5,897,537 (2018 dollars).

Ideally, building improvements would occur at the same time to potentially save Spanish Fork money due to inflation over the long term. However, we recognize the budgeting requirements in Spanish Fork and the need to prioritize the elements that should be built first.

We have separated the various costs for the design elements throughout Main Street to assist the City in identifying what to build first. For a detailed breakdown of costs see Appendix B. The approximate cost for the Main Street corridor project, from I-15 to Arrowhead Trail, is broken into six key areas:

Area 1: The Main Street Downtown Core from 400 North to 100 South

Approximate cost = \$1,819,384

Area 2: Transition Zone North (700 North to 400 North)

Approximate cost = \$1,091,822

Area 3: Transition Zone South (100 South to 300 South)

Approximate cost = \$695,568

Area 4: I-15 Commercial Zone (I-15 to 700 North)

Approximate cost = \$1,090,268

Area 5: Fairgrounds Zone (300 South to Volunteer Drive)

Approximate cost = \$992,917

Area 6: River Zone (Volunteer Drive to Arrowhead Trail)

Approximate cost = \$207,578

Priority of Improvements

First Priority: Most of the focus throughout the concept development process was spent on the Downtown Core area from 400 North to 100 South. For this reason, the highest priority should be given to the construction of this area. Most businesses are located in this area and will reap the most benefit from an improved streetscape. It is in the best interest of the City that these five key blocks from 400 North to 100 South provide a catalyst for the development of the remainder of the Main Street corridor.

Second Priority: Transition zones to the north and south are a secondary priority. Improvements will include the installation of center landscaped medians, lighting, additional or improved transit stops, and improved crosswalks. Existing parkstrips in transition areas will remain turf or water-wise landscaping.

Third Priority: The I-15 Commercial Zone and the Fairgrounds Zone are a third priority for construction improvements. The suggested improvements for the Commercial Zone include a center landscaped median, lighting, and some light landscaping, however for the Fairgrounds Zone additional traffic lights are recommended to better control traffic in and out of the Fairgrounds. Roadway improvements will include new striping but no new paving materials.

Fourth Priority: The River Zone is a periphery area which is currently in a more rural area of the City. Little to no improvement, such as installation of sidewalks, lighting, and some light landscaping, is recommended for this area. Roadway improvements will include new striping but no new paving materials.

Construction Schedule

To create the Main Street Vision Plan, a full reconstruction of Main Street will be required. First, the existing sewer and water lines should be replaced or upgraded. This may be accomplished with pipe-bursting (trenchless technology) or through open trenching. It is key to replace the utilities first. Once the roadway work is completed, UDOT will place a seven-year moratorium on roadway cuts in the asphalt. Following the utility replacements, curb and gutter along with storm drainage improvements will be necessary. This would include the installation of Low Impact Development (LID) best management practices (BMPs), such as curb cuts, to develop rain gardens in the landscape areas. Additionally, this would be the best time to install lighting and irrigation conduits. Once these are done, the new sidewalks with sitting areas and planters could be constructed. Finally, Main Street would be repaved as part of the UDOT project. This would finalize the Main Street improvements with a fresh driving surface, consistent parking widths and sidewalk widths, and clear traffic striping.

If at all possible, constructing the improvements together is ideal to reduce costs. One example of cost saving measures would be in traffic control. Traffic control during construction is a variable not included in the cost estimating figures provided. By combining the improvement schedules, traffic control costs could be reduced dramatically. Overall, there are many advantages to combining the construction schedule, such as reduced costs, less impact to the traveling public, and quicker economic development in the downtown area.

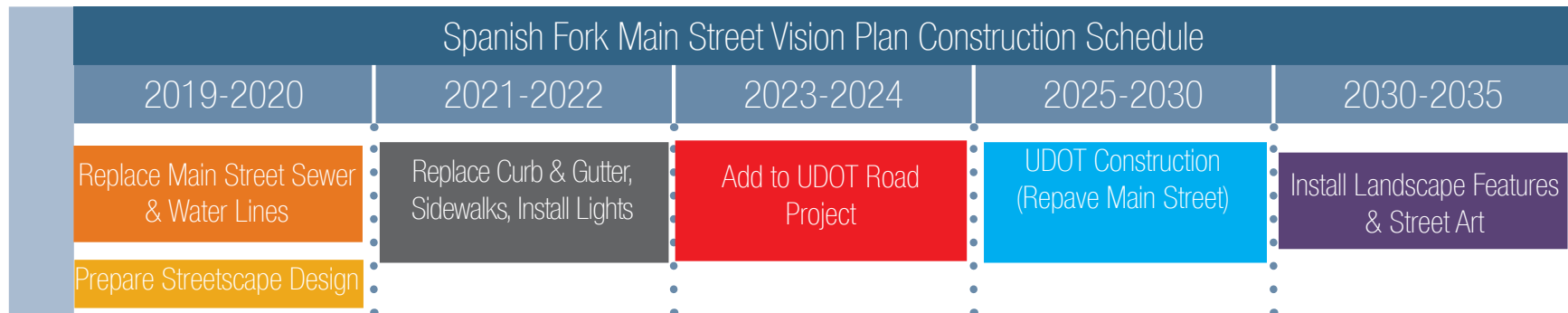


Table 5.1 Proposed Construction Schedule

Funding Websites

TIGER: <https://www.transportation.gov/tiger>

MAG: <https://www.mountainland.org/>

HUD Main Street Grants: http://portal.hud.gov/hudportal/HUD?src=/program_offices/public_indian_housing/programs/ph/hope6/grants/mainstreet

Funding Options

Various funding options, through many sources, are available for the fulfillment of the proposed improvements for the Spanish Fork Main Street Vision Plan. A few funding programs are listed below. However, these are merely suggestions for potential funding sources, and the list is not exhaustive. To prevent wasted efforts in seeking funding, discussions with the funding agencies will be necessary before starting the application process.

TIGER

Transportation Investment Generating Economic Recovery (TIGER) is a supplementary discretionary grant program that was included in the American Recovery and Reinvestment Act of 2009. With \$1.5 billion available for a National Surface Transportation System, the money is awarded on a competitive basis for surface transportation projects. Investment in these types of projects helps with job creation. According to the U.S. Department of Transportation, applicants who are eligible to receive funding for surface transportation projects include:

- State and local governments, including U.S. territories and regional tribal councils
- Transit agencies
- Port authorities
- Metropolitan planning organizations (MPOs)
- Multi-state or multi-jurisdictional applicants

TIGER 2015: More than 600 applicants requested \$9.8 billion worth of projects; the projects included road projects, transit projects, rail projects, and port and bicycle and pedestrian projects.



Figure 5.1 Springville received funding through MAG to modify Main Street to revitalize the downtown area.

MAG

Mountainland Association of Governments (MAG) is an association of government agencies that serves as an advisory only, for Utah and Wasatch counties by providing community and business development, regional planning, and senior resources and services. MAG accesses funding from several sources, including sales tax revenue and transportation (gas tax) revenue (See Figure 5.1).

HUD Main Street Grants

The U.S. Department of Housing and Urban Development (HUD) Main Street program seeks to rejuvenate older, downtown business districts while retaining the area's traditional and historic character. The Main Street program assists smaller communities in the development of affordable housing that is undertaken in connection with a Main Street revitalization effort. For example, obsolete commercial offices or buildings can be reconfigured into rent producing affordable housing. Only units of general local government are eligible to apply. The primary eligibility requirements are that the applicant

- Be a local government (subdivision of a state or territory);
- Have a population of 50,000 or less within its jurisdiction; and
- Have 100 or fewer physical public housing units within its jurisdiction.



Figure 5.2 Brigham City before facade and streetscape improvements.



Figure 5.3 Brigham City after facade and streetscape improvements.

Recommendations

The project team reviewed the data from area mapping, public input, and design discussions to compile recommendations for the Main Street Study.

- The team recommends pursuing rear building parking before 2021 to meet the proposed schedule.
- Support a branding theme reinforcing the historic, natural, and cultural elements that make Spanish Fork unique. Work with Utah County and the Utah Office of Tourism to support branding. Incorporate the branding philosophy into all aspects including permanent and temporary art as well as landscape and hardscape elements.
- Create Facade Grants for Main Street business owners to complete front and rear aesthetic improvements. The grants would be post performance issued for 50% of construction costs not to exceed \$10,000. Consider adding a CDA to promote better financing and improvement options.
- Construct low maintenance medians and intersection bump-outs to psychologically slow down drivers and integrate a pedestrian crossing refuge to increase safety and walkability.
- Create attractive public parking lots that include way finding signs, lighting, and business access. Formalize agreements with large parking lot owners for shared use. Consider establishing a parking district to finance improvements and help manage the system.
- Although Fiesta Days and Icelandic Days are very popular, there are insufficient activities to bring people downtown on a regular basis. Planning organizations, such as the APA, recommend that to increase patronage and tourism, the number of year round activities gradually but continuously build. Based on other communities of similar size the optimum number is 200 to 250 days per year of scheduled activities.



Figure 5.4 SOHO Food Park in Holladay, Utah brought the community together on a reclaimed triangular piece of their downtown.



Figure 5.5 The popularity of Spanish Fork's splash pad prompts discussion on adding an additional water feature amenity to the Downtown Core.

- To accommodate existing and increased activities for the Downtown Area, it is recommended that the Library Block be redesigned to include a dedicated outdoor space for events. Area improvements may include a food truck area, water feature/splash pad, and/or an informal amphitheater.
- Actively pursue a mix of businesses and hours of operation conducive to shopping, entertainment, and dining. Encourage businesses to remain open when more people shop and provide public restroom access. Due to our ever changing lifestyles, 70% of retail purchases now occur after 6:00 p.m. In order to achieve critical mass as a destination location, set a goal of ten businesses in each of the following categories: 10 businesses that sell food (restaurants, cafes, coffee shops, etc.), 10 non-chain destination retail shops (galleries, antique stores, collectibles, books, home accents, outfitters, etc.), and 10 businesses open after 6:00 p.m. (entertainment, theaters, arts, specialty shops, dining, etc.).
- Upgrade zoning ordinances to address appropriate land uses, site planning, and historic design. Address signs, murals, sidewalk displays, and shared use for upper stories such as residential or office space over street level retail space.

APPENDIX A

Design Materials



Figure 1. Combination of different concrete finishes. By adding an exposed aggregate ribbon the hardscape picks up the river theme throughout the project.



Figure 2. Adding a stain or Natina concrete color treatment adds additional accentuation to the hardscape.



Figure 3. Using 4" to 6" cobble to imitate a river bed ribbon throughout the Main Street corridor will represent the influence of the Spanish Fork River on the community and its history.



Figure 4. A smaller rock mulch or bark mulch will be used in the adjacent landscape areas.



Figure 5. Regularly spaced curb cuts allow storm water to be harvested into landscape bed areas.



Figure 6. Colored and stamped asphalt used for crosswalks offer a visual delineation from the main roadway traffic.



Rose



Sweetgum Tree leaves



Pear Tree leaves



Feather Reed Grass



Dwarf Hameln Grass



Dwarf Dogwood

Figure 14. Landscape grasses and shrubs provide color and texture to planter areas and will soften the hardscape of the streetscape.



Salvia



Daylily



Sedum



Spirea

APPENDIX B

Cost Summaries

Cost Estimate—Downtown Core (400 North to 100 South)

Materials	Units	Cost/Unit	Quantity	Total Cost
Concrete Curb and gutter & Drive Approaches	Sq. Ft.	\$ 40.00	16,595	\$ 663,800.00
Concrete Curb Wall in Medians (materials and labor)	Sq. Ft.	\$ 40.00	2,177	\$ 87,078.00
Sidewalk (concrete and labor)	Sq. Ft.	\$ 6.00	42,580	\$ 255,480.00
Sidewalk (aggregate with stain)	Sq. Ft.	\$ 10.00	17,485	\$ 174,850.00
ADA Ramps	Each	\$ 2,500.00	21	\$ 52,500.00
Asphalt Paving (stamping)	Sq. Ft.	\$ 3.00	9,355	\$ 28,065.00
Off Street Parking (striping)	Linear Ft.	\$ 2.15	720	\$ 1,548.00
Crosswalk (striping)	Linear Ft.	\$ 1.00	3,594	\$ 3,594.00
Bike Lane (striping)	Linear Ft.	\$ 1.00	9,740	\$ 9,740.00
TOTAL				\$ 1,276,655.00
Landscape	Units	Cost/Unit	Quantity	Total Cost
<i>Shrubs</i>	Each	\$ 35.00	770	\$ 26,950.00
<i>Grasses</i>	Each	\$ 25.00	400	\$ 10,000.00
<i>Trees (2" caliper)</i>	Each	\$ 350.00	131	\$ 45,850.00
<i>Bed area (2" cobble rock)</i>	Cu. Yard	\$ 25.00	311	\$ 7,775.00
<i>Bed area (4"-6" cobble rock)</i>	Cu. Yard	\$ 10.00	195	\$ 1,950.00
<i>Topsoil (12" depth)</i>	Cu. Yard	\$ 10.00	1400	\$ 14,000.00
<i>Irrigation</i>	Sq. Ft.	\$ 1.25	37,763	\$ 47,203.75
TOTAL				\$ 153,728.75
Lighting	Units	Cost/Unit	Quantity	Total Cost
<i>24 ft pole</i>	Each	\$ 4,500.00	30	\$ 135,000.00
<i>18 ft pole</i>	Each	\$ 3,800.00	40	\$ 152,000.00
TOTAL				\$ 287,000.00
Site Furnishings	Units	Cost/Unit	Quantity	Total Cost
<i>Benches</i>	Each	\$ 800.00	40	\$ 32,000.00
<i>Banners</i>	Each	\$ 100.00	70	\$ 7,000.00
<i>Trash Receptacles</i>	Each	\$ 350.00	20	\$ 7,000.00
<i>Wayfinding Signs</i>	Each	\$ 800.00	10	\$ 8,000.00
<i>Entrance Signs</i>	Each	\$ 1,500.00	2	\$ 3,000.00
<i>Art Pieces</i>	Each	\$ 15,000.00	3	\$ 45,000.00
TOTAL				\$ 102,000.00
TOTAL COST				\$ 1,819,383.75

Cost Estimate—Transition North

Materials				
	Units	Cost/Unit	Quantity	Total Cost
Concrete Curb and gutter & Drive Approaches	Sq. Ft.	\$ 40.00	13,724	\$ 548,960.00
Concrete Curb Wall in Medians (materials and labor)	Sq. Ft.	\$ 40.00	1,306	\$ 52,240.00
Sidewalk (concrete and labor)	Sq. Ft.	\$ 6.00	16,941	\$ 101,646.00
Sidewalk (aggregate with stain)	Sq. Ft.	\$ 10.00	6,899	\$ 68,990.00
ADA Ramps	Each	\$ 2,500.00	12	\$ 30,000.00
Asphalt Paving (stamping)	Sq. Ft.	\$ 3.00	5,879	\$ 17,637.00
Off Street Parking (striping)	Linear Ft.	\$ 2.15	90	\$ 193.50
Crosswalk (striping)	Linear Ft.	\$ 1.00	1,542	\$ 1,542.00
Bike Lane (striping)	Linear Ft.	\$ 1.00	5,584	\$ 5,584.00
TOTAL				\$ 826,792.50
Landscape				
	Units	Cost/Unit	Quantity	Total Cost
<i>Shrubs</i>	Each	\$ 35.00	770	\$ 26,950.00
<i>Grasses</i>	Each	\$ 25.00	400	\$ 10,000.00
<i>Trees (2" caliper)</i>	Each	\$ 350.00	66	\$ 23,100.00
<i>Bed area (2" cobble rock)</i>	Cu. Yard	\$ 25.00	307	\$ 7,675.00
<i>Bed area (4"-6" cobble rock)</i>	Cu. Yard	\$ 10.00	188	\$ 1,880.00
<i>Topsoil (12" depth)</i>	Cu. Yard	\$ 10.00	1,297	\$ 12,970.00
<i>Irrigation</i>	Sq. Ft.	\$ 1.25	35,004	\$ 43,755.00
TOTAL				\$ 126,330.00
Lighting				
	Units	Cost/Unit	Quantity	Total Cost
<i>24 ft pole</i>	Each	\$ 4,500.00	12	\$ 54,000.00
<i>18 ft pole</i>	Each	\$ 3,800.00	18	\$ 68,400.00
TOTAL				\$ 122,400.00
Site Furnishings				
	Units	Cost/Unit	Quantity	Total Cost
<i>Benches</i>	Each	\$ 800.00	12	\$ 9,600.00
<i>Banners</i>	Each	\$ 100.00	30	\$ 3,000.00
<i>Trash Receptacles</i>	Each	\$ 350.00	6	\$ 2,100.00
<i>Wayfinding Signs</i>	Each	\$ 800.00	2	\$ 1,600.00
TOTAL				\$ 16,300.00
TOTAL COST				\$ 1,091,822.50

Cost Estimate—Transition South

Materials				
	Units	Cost/Unit	Quantity	Total Cost
Concrete Curb and gutter & Drive Approaches	Sq. Ft.	\$ 40.00	8,130	\$ 325,200.00
Concrete Curb Wall in Medians (materials and labor)	Sq. Ft.	\$ 40.00	876	\$ 35,040.00
Sidewalk (concrete and labor)	Sq. Ft.	\$ 6.00	10,998	\$ 65,988.00
Sidewalk (aggregate with stain)	Sq. Ft.	\$ 10.00	5,027	\$ 50,270.00
ADA Ramps	Each	\$ 2,500.00	8	\$ 20,000.00
Asphalt Paving (stamping)	Sq. Ft.	\$ 3.00	4,320	\$ 12,960.00
Off Street Parking (striping)	Linear Ft.	\$ 2.15	72	\$ 154.80
Crosswalk (striping)	Linear Ft.	\$ 1.00	1,107	\$ 1,107.00
Bike Lane (striping)	Linear Ft.	\$ 1.00	3,824	\$ 3,824.00
TOTAL				\$ 514,543.80
Landscape				
	Units	Cost/Unit	Quantity	Total Cost
<i>Shrubs</i>	Each	\$ 35.00	770	\$ 26,950.00
<i>Grasses</i>	Each	\$ 25.00	400	\$ 10,000.00
<i>Trees (2" caliper)</i>	Each	\$ 350.00	20	\$ 7,000.00
<i>Bed area (2" cobble rock)</i>	Cu. Yard	\$ 25.00	214	\$ 5,350.00
<i>Bed area (4"-6" cobble rock)</i>	Cu. Yard	\$ 10.00	106	\$ 1,060.00
<i>Topsoil (12" depth)</i>	Cu. Yard	\$ 10.00	852	\$ 8,520.00
<i>Irrigation</i>	Sq. Ft.	\$ 1.25	23,035	\$ 28,793.75
TOTAL				\$ 87,673.75
Lighting				
	Units	Cost/Unit	Quantity	Total Cost
<i>24 ft pole</i>	Each	\$ 4,500.00	8	\$ 36,000.00
<i>18 ft pole</i>	Each	\$ 3,800.00	12	\$ 45,600.00
TOTAL				\$ 81,600.00
Site Furnishings				
	Units	Cost/Unit	Quantity	Total Cost
<i>Benches</i>	Each	\$ 800.00	8	\$ 6,400.00
<i>Banners</i>	Each	\$ 100.00	20	\$ 2,000.00
<i>Trash Receptacles</i>	Each	\$ 350.00	5	\$ 1,750.00
<i>Wayfinding Signs</i>	Each	\$ 800.00	2	\$ 1,600.00
TOTAL				\$ 11,750.00
TOTAL COST				\$ 695,567.55

Cost Estimate - I-15 Commercial

Materials	Units	Cost/Unit	Quantity	Total Cost
Concrete Curb and gutter & Drive Approaches	Sq. Ft.	\$ 40.00	12,626	\$ 505,056.00
Concrete Curb Wall in Medians (materials and labor)	Sq. Ft.	\$ 40.00	2,461	\$ 98,440.00
Sidewalk (concrete and labor)	Sq. Ft.	\$ 6.00	16,965	\$ 101,790.00
Sidewalk (aggregate with stain)	Sq. Ft.	\$ 10.00	7,377	\$ 73,770.00
ADA Ramps	Each	\$ 2,500.00	14	\$ 35,000.00
Asphalt Paving (stamping)	Sq. Ft.	\$ 3.00	7,790	\$ 23,370.00
Off Street Parking (striping)	Linear Ft.	\$ 2.15	27	\$ 58.05
Crosswalk (striping)	Linear Ft.	\$ 1.00	1,880	\$ 1,880.00
Bike Lane (striping)	Linear Ft.	\$ 1.00	8,056	\$ 8,056.00
TOTAL				\$ 847,420.05
Landscape	Units	Cost/Unit	Quantity	Total Cost
<i>Shrubs</i>	Each	\$ 35.00	770	\$ 26,950.00
<i>Grasses</i>	Each	\$ 25.00	400	\$ 10,000.00
<i>Trees (2" caliper)</i>	Each	\$ 350.00	63	\$ 22,050.00
<i>Bed area (2" cobble rock)</i>	Cu. Yard	\$ 25.00	267	\$ 6,675.00
<i>Bed area (4"-6" cobble rock)</i>	Cu. Yard	\$ 10.00	172	\$ 1,720.00
<i>Topsoil (12" depth)</i>	Cu. Yard	\$ 10.00	1140	\$ 11,400.00
<i>Irrigation</i>	Sq. Ft.	\$ 1.25	30,802	\$ 38,502.50
TOTAL				\$ 117,297.50
Lighting	Units	Cost/Unit	Quantity	Total Cost
24 ft pole	Each	\$ 4,500.00	24	\$ 108,000.00
TOTAL				\$ 108,000.00
Site Furnishings	Units	Cost/Unit	Quantity	Total Cost
<i>Benches</i>	Each	\$ 800.00	12	\$ 9,600.00
<i>Banners</i>	Each	\$ 100.00	24	\$ 2,400.00
<i>Trash Receptacles</i>	Each	\$ 350.00	7	\$ 2,450.00
<i>Wayfinding Signs</i>	Each	\$ 800.00	2	\$ 1,600.00
<i>Entrance Signs</i>	Each	\$ 1,500.00	1	\$ 1,500.00
TOTAL				\$ 17,550.00
TOTAL COST				\$ 1,090,267.55

Cost Estimate - Fairgrounds

Materials	Units	Cost/Unit	Quantity	Total Cost
Concrete Curb and gutter & Drive Approaches	Sq. Ft.	\$ 40.00	11,475	\$ 459,000.00
Concrete Curb Wall in Medians (materials and labor)	Sq. Ft.	\$ 40.00	408	\$ 16,320.00
Sidewalk (concrete and labor)	Sq. Ft.	\$ 6.00	13,929	\$ 83,574.00
Sidewalk (aggregate with stain)	Sq. Ft.	\$ 10.00	7,958	\$ 79,580.00
ADA Ramps	Each	\$ 2,500.00	11	\$ 27,500.00
Asphalt Paving (stamping)	Sq. Ft.	\$ 3.00	2,721	\$ 8,163.00
Off Street Parking (striping)	Linear Ft.	\$ 2.15	99	\$ 212.85
Crosswalk (striping)	Linear Ft.	\$ 1.00	672	\$ 672.00
Bike Lane (striping)	Linear Ft.	\$ 1.00	11,236	\$ 11,236.00
TOTAL				\$ 686,257.85
Landscape	Units	Cost/Unit	Quantity	Total Cost
<i>Shrubs</i>	Each	\$ 35.00	770	\$ 26,950.00
<i>Grasses</i>	Each	\$ 25.00	400	\$ 10,000.00
<i>Trees (2" caliper)</i>	Each	\$ 350.00	150	\$ 52,500.00
<i>Bed area (2" cobble rock)</i>	Cu. Yard	\$ 25.00	375	\$ 9,375.00
<i>Bed area (4"-6" cobble rock)</i>	Cu. Yard	\$ 10.00	260	\$ 2,600.00
<i>Topsoil (12" depth)</i>	Cu. Yard	\$ 10.00	1,644	\$ 16,440.00
<i>Irrigation</i>	Sq. Ft.	\$ 1.25	44,475	\$ 55,593.75
TOTAL				\$ 173,458.75
Lighting	Units	Cost/Unit	Quantity	Total Cost
24 ft pole	Each	\$ 4,500.00	24	\$ 108,000.00
TOTAL				\$ 108,000.00
Site Furnishings	Units	Cost/Unit	Quantity	Total Cost
<i>Benches</i>	Each	\$ 800.00	6	\$ 4,800.00
<i>Banners</i>	Each	\$ 100.00	24	\$ 2,400.00
<i>Trash Receptacles</i>	Each	\$ 350.00	4	\$ 1,400.00
<i>Wayfinding Signs</i>	Each	\$ 800.00	2	\$ 1,600.00
<i>Art Pieces</i>	Each	\$ 15,000.00	1	\$ 15,000.00
TOTAL				\$ 25,200.00
TOTAL COST				\$ 992,916.60

Cost Estimate - River

Materials		Units	Cost/Unit	Quantity	Total Cost
	Sidewalk (concrete and labor)	Sq. Ft.	\$ 6.00	8,483	\$ 50,898.00
	Sidewalk (aggregate with stain)	Sq. Ft.	\$ 10.00	1,856	\$ 18,560.00
	ADA Ramps	Each	\$ 2,500.00	3	\$ 7,500.00
	Asphalt Paving (stamping)	Sq. Ft.	\$ 3.00	1,420	\$ 4,260.00
	Crosswalk (striping)	Linear Ft.	\$ 1.00	350	\$ 350.00
	Bike Lane (striping)	Linear Ft.	\$ 1.00	5,848	\$ 5,848.00
	TOTAL				\$ 87,416.00
Landscape		Units	Cost/Unit	Quantity	Total Cost
	<i>Shrubs</i>	Each	\$ 35.00	350	\$ 12,250.00
	<i>Grasses</i>	Each	\$ 25.00	150	\$ 3,750.00
	<i>Trees (2" caliper)</i>	Each	\$ 350.00	47	\$ 16,450.00
	<i>Bed area (2" cobble rock)</i>	Cu. Yard	\$ 25.00	58	\$ 1,450.00
	<i>Bed area (4"-6" cobble rock)</i>	Cu. Yard	\$ 10.00	48	\$ 480.00
	<i>Topsoil (12" depth)</i>	Cu. Yard	\$ 10.00	268	\$ 2,680.00
	<i>Irrigation</i>	Sq. Ft.	\$ 1.25	7,202	\$ 9,002.50
	TOTAL				\$ 46,062.50
Lighting		Units	Cost/Unit	Quantity	Total Cost
	<i>24 ft pole</i>	Each	\$ 4,500.00	12	\$ 54,000.00
	TOTAL				\$ 54,000.00
Site Furnishings		Units	Cost/Unit	Quantity	Total Cost
	<i>Benches</i>	Each	\$ 800.00	2	\$ 1,600.00
	<i>Banners</i>	Each	\$ 100.00	12	\$ 1,200.00
	<i>Trash Receptacles</i>	Each	\$ 350.00	2	\$ 700.00
	<i>Wayfinding Signs</i>	Each	\$ 800.00	2	\$ 1,600.00
	<i>Art Pieces</i>	Each	\$ 15,000.00	1	\$ 15,000.00
	TOTAL				\$ 20,100.00
TOTAL COST					\$ 207,578.50

APPENDIX C

Public Involvement Materials



Demographics: City of Residence

QUESTION 1

QUESTION 2

QUESTION 3

QUESTION 4

QUESTION 5

QUESTION 6

QUESTION 7

QUESTION 8

QUESTION 9

QUESTION 10

QUESTION 11

QUESTION 12

QUESTION 13

QUESTION 14

QUESTION 15

QUESTION 16

QUESTION 17

QUESTION 18

QUESTION 19

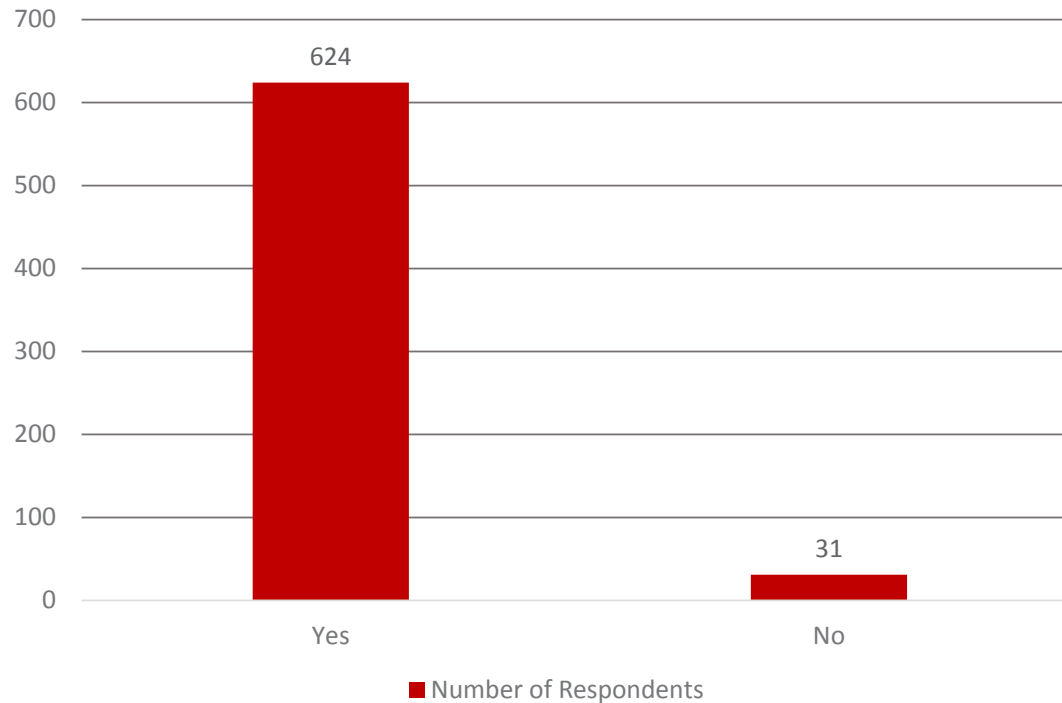
QUESTION 20

QUESTION 21

QUESTION 22

QUESTION 23

Q1: Do you currently live or work in Spanish Fork?

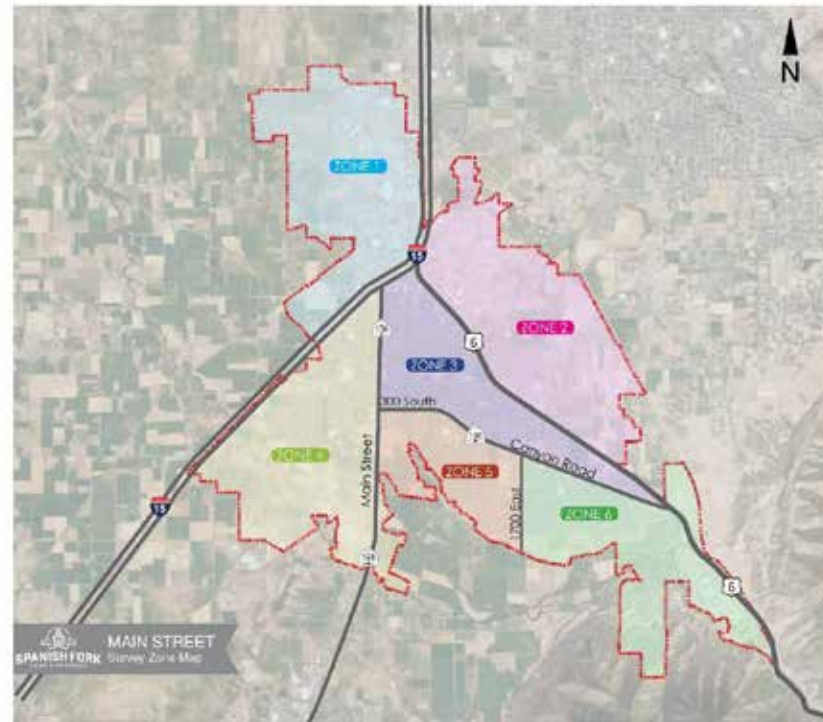




Demographics: Area of Residence

- QUESTION 1
- QUESTION 2**
- QUESTION 3
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- QUESTION 23

Q2: According to the image below, in which zone of Spanish Fork do you live/work?

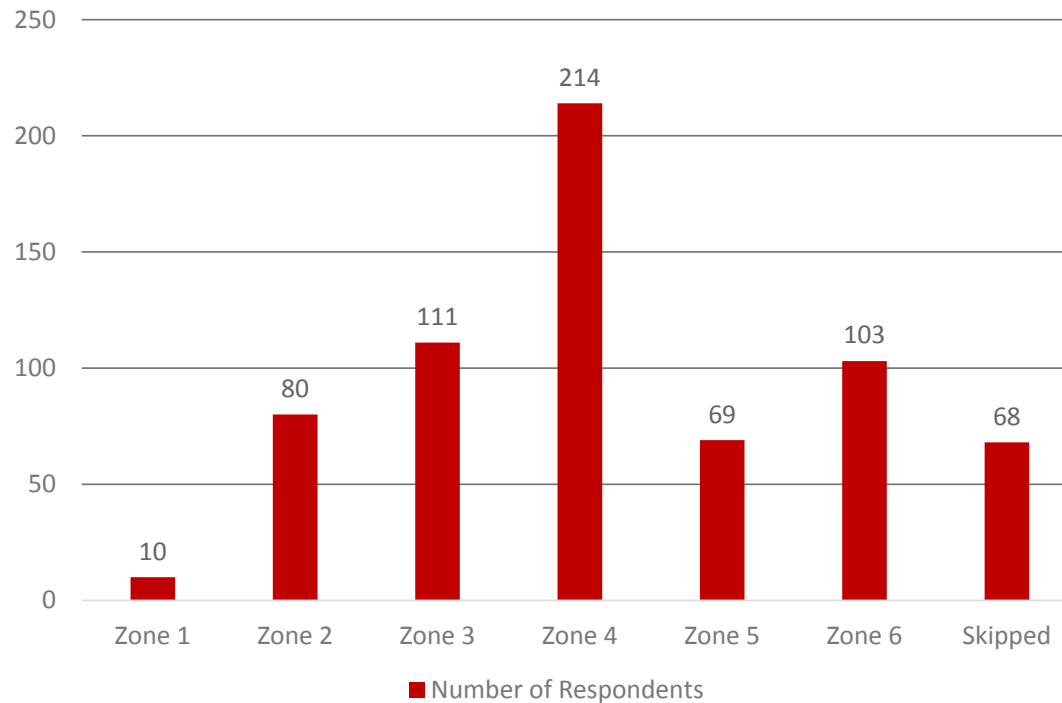




Demographics: Area of Residence

- QUESTION 1
- QUESTION 2**
- QUESTION 3
- QUESTION 4
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- QUESTION 23

Q2: According to the image below, in which zone of Spanish Fork do you live/work?

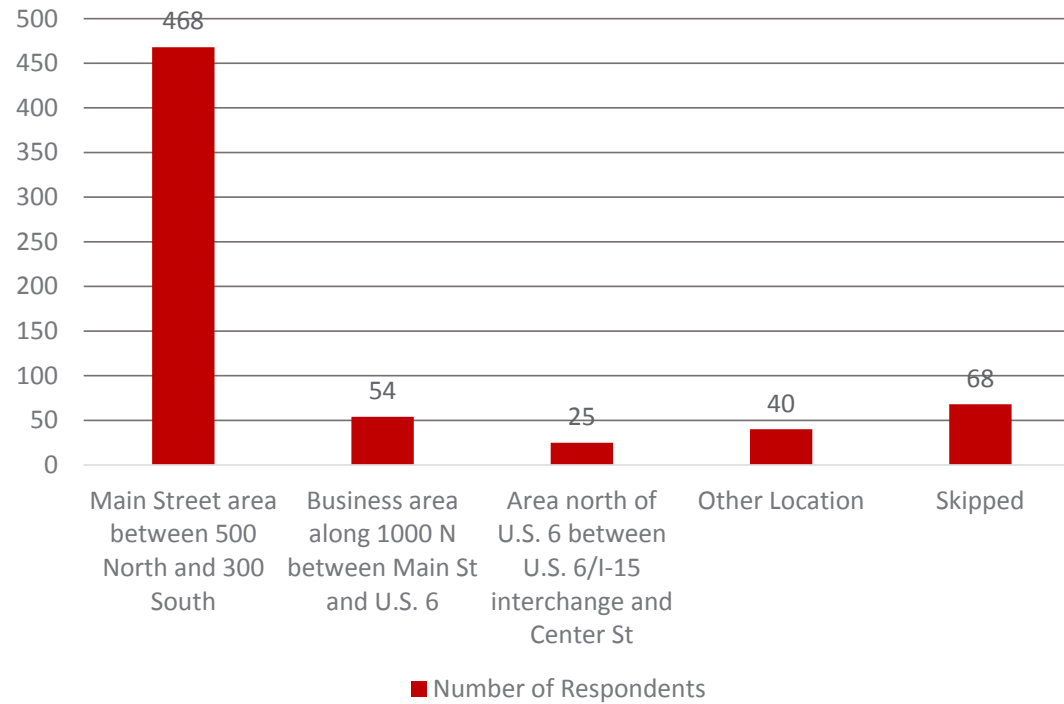




Place: Downtown Area

- QUESTION 1
- QUESTION 2
- QUESTION 3**
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- QUESTION 22
- QUESTION 23

Q3: I define the downtown area of Spanish Fork City as:

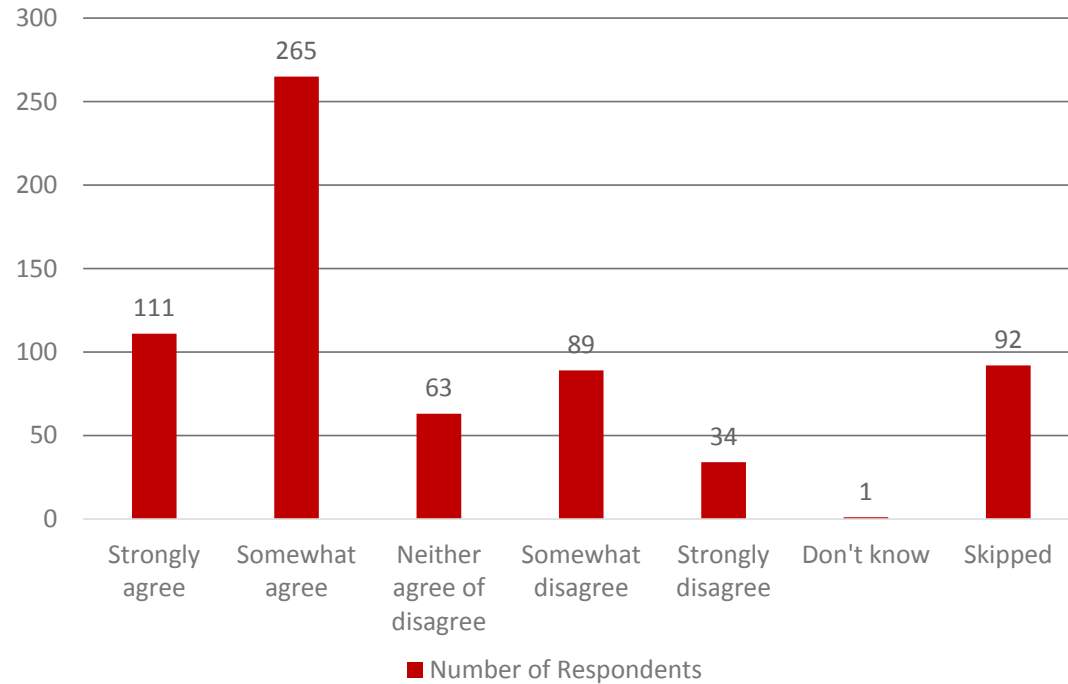




Use: Downtown Activities

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4**
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- QUESTION 8
- QUESTION 9
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- QUESTION 23

Q4: Please tell us how much you either agree or disagree with the following statement about Spanish Fork City: I consider the downtown area of Spanish Fork Main Street to be a destination for me to shop, work, and attend other activities?

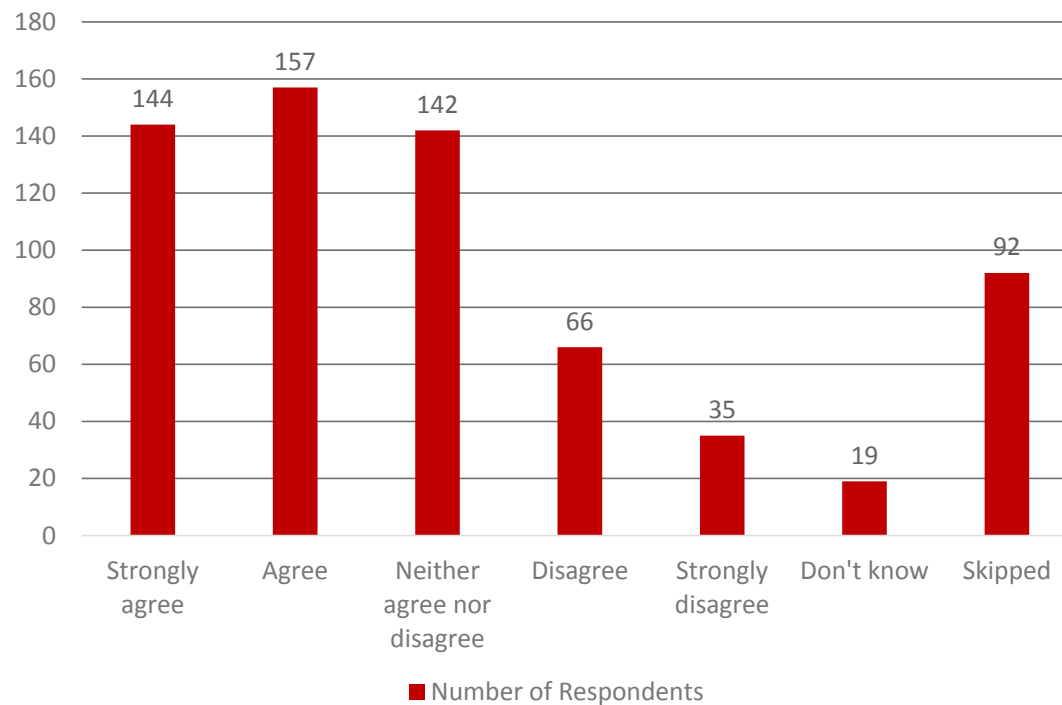




Use: Main Commuter Routes

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
- QUESTION 5**
- QUESTION 6
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- QUESTION 8
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- QUESTION 23

Q5: I would visit the Spanish Fork Main Street area if the road functioned more like a local road instead of a primary commuter route?

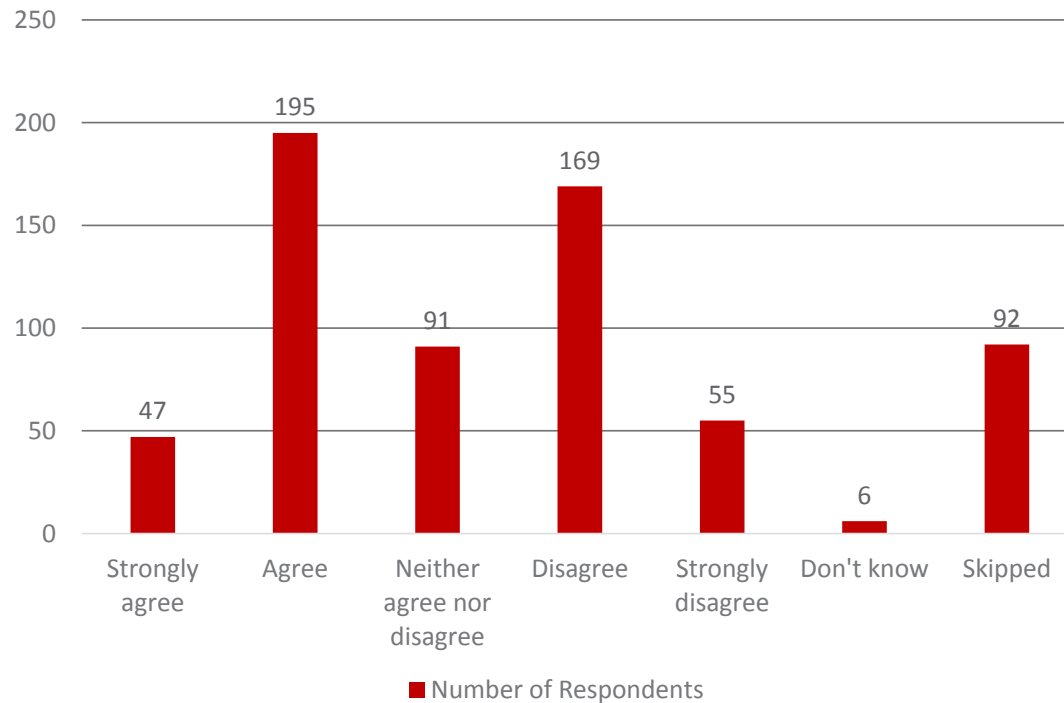




Use: Walkability

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
- QUESTION 5
- QUESTION 6**
- QUESTION 7
- QUESTION 8
- QUESTION 9
- QUESTION 10
- QUESTION 11
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- QUESTION 22
- QUESTION 23

Q6: The Spanish Fork Main Street area is pedestrian friendly and walkable?

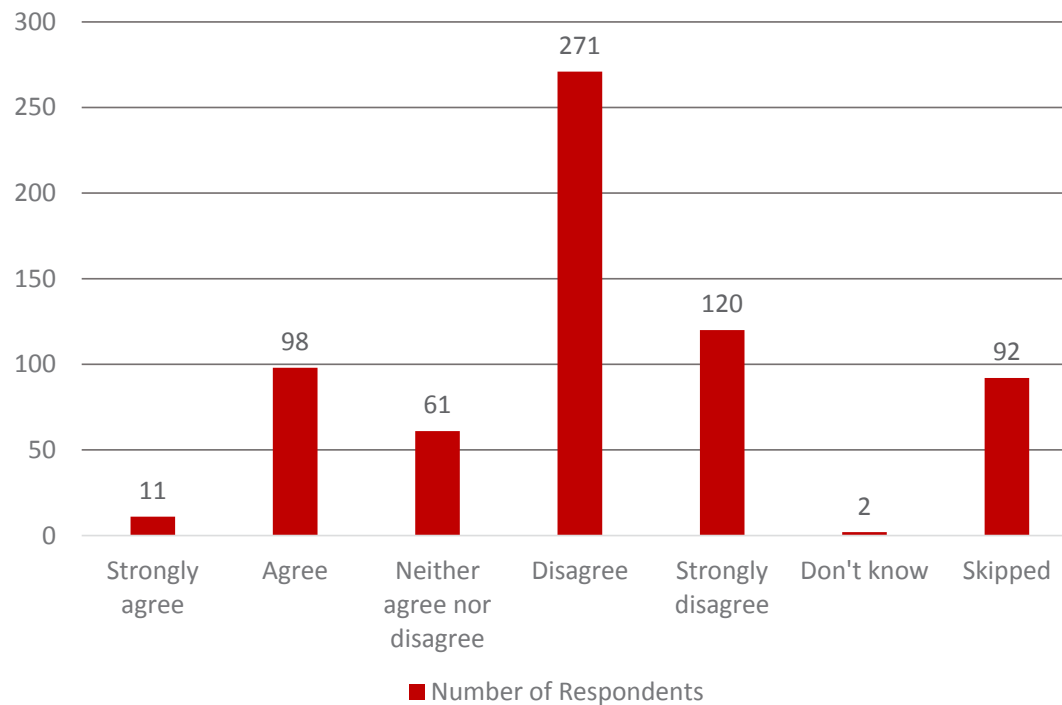




Use: Parking Availability

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
- QUESTION 5
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- QUESTION 7**
- QUESTION 8
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- QUESTION 22
- QUESTION 23

Q7: The Spanish Fork Main Street area provides sufficient, available parking?

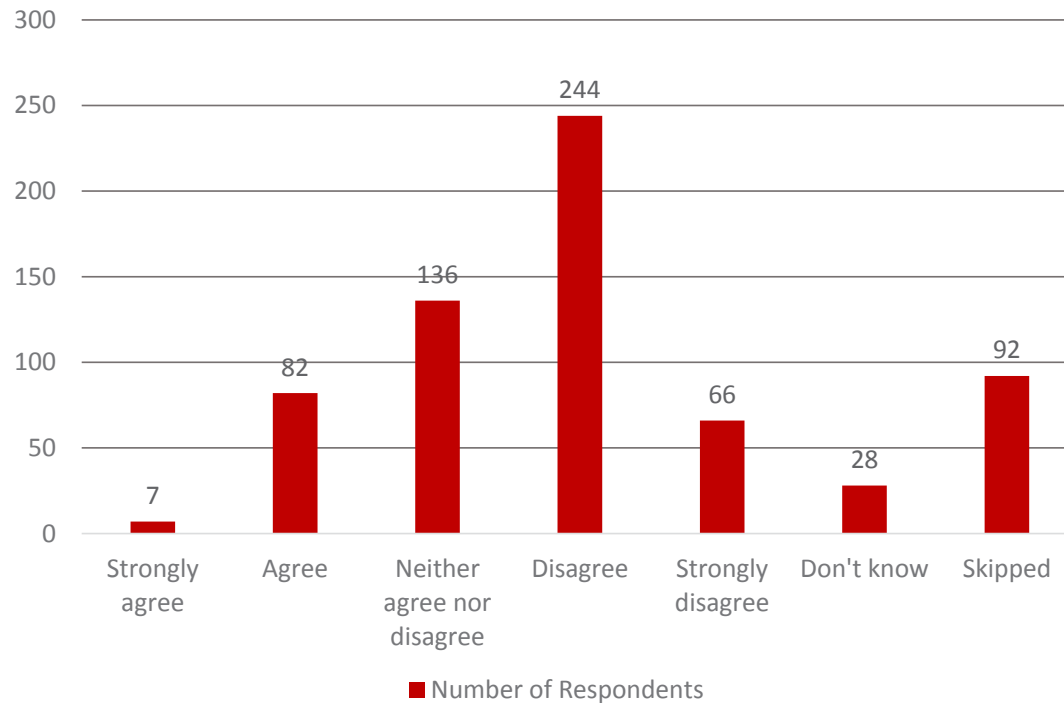




Use: Modes of Travel

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
- QUESTION 5
- QUESTION 6
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- QUESTION 8**
- QUESTION 9
- QUESTION 10
- QUESTION 11
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- QUESTION 21
- QUESTION 22
- QUESTION 23

Q8: The Spanish Fork Main Street area provides good, sufficient access to use different modes of travel such as biking and transit?

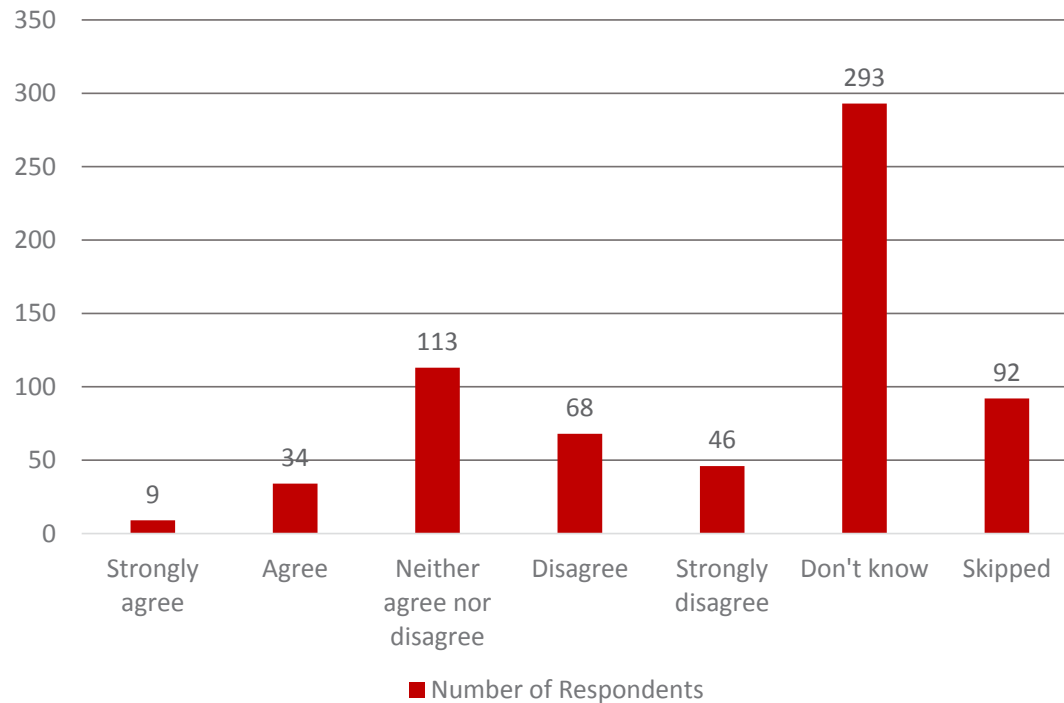




Use: Bus System

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
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- QUESTION 8
- QUESTION 9**
- QUESTION 10
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- QUESTION 22
- QUESTION 23

Q9: The current bus system on Main Street provides good, efficient service that meets my travel needs?

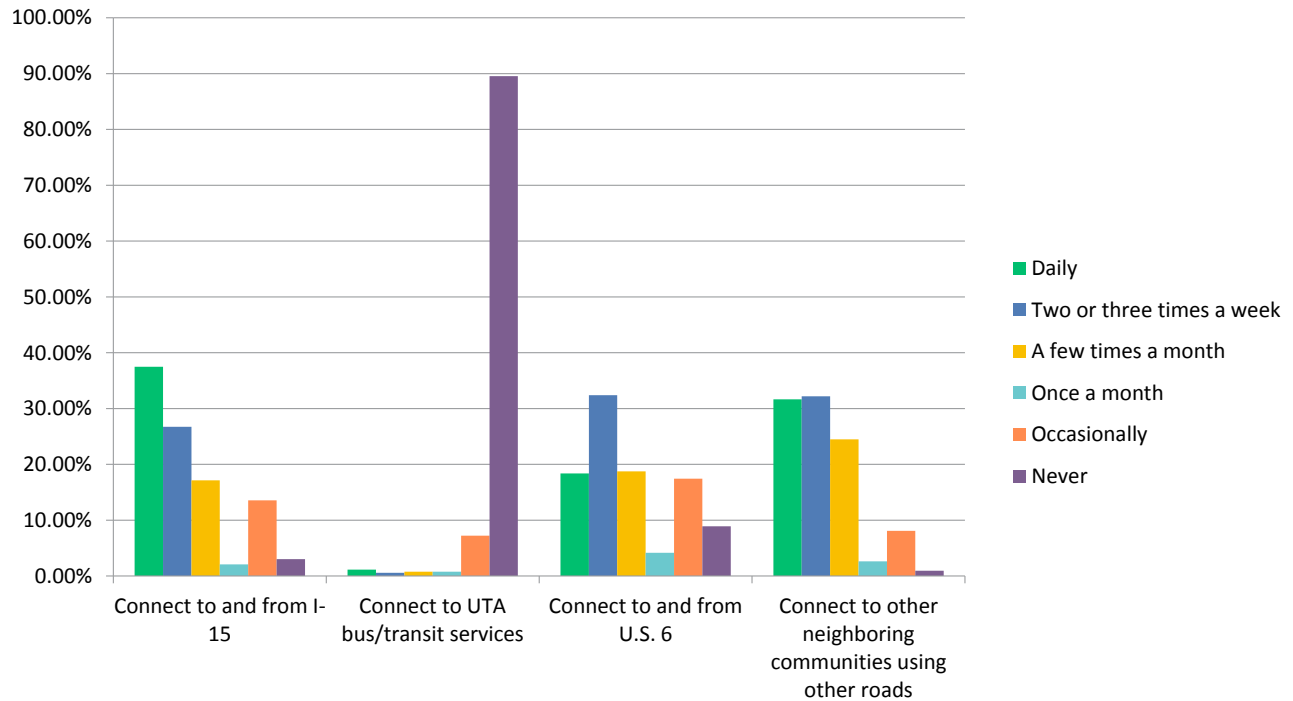




Use: Frequency of Use

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
- QUESTION 5
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- QUESTION 8
- QUESTION 9
- QUESTION 10**
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- QUESTION 22
- QUESTION 23

Q10: Please tell us how frequently you travel on Main Street to do the following:

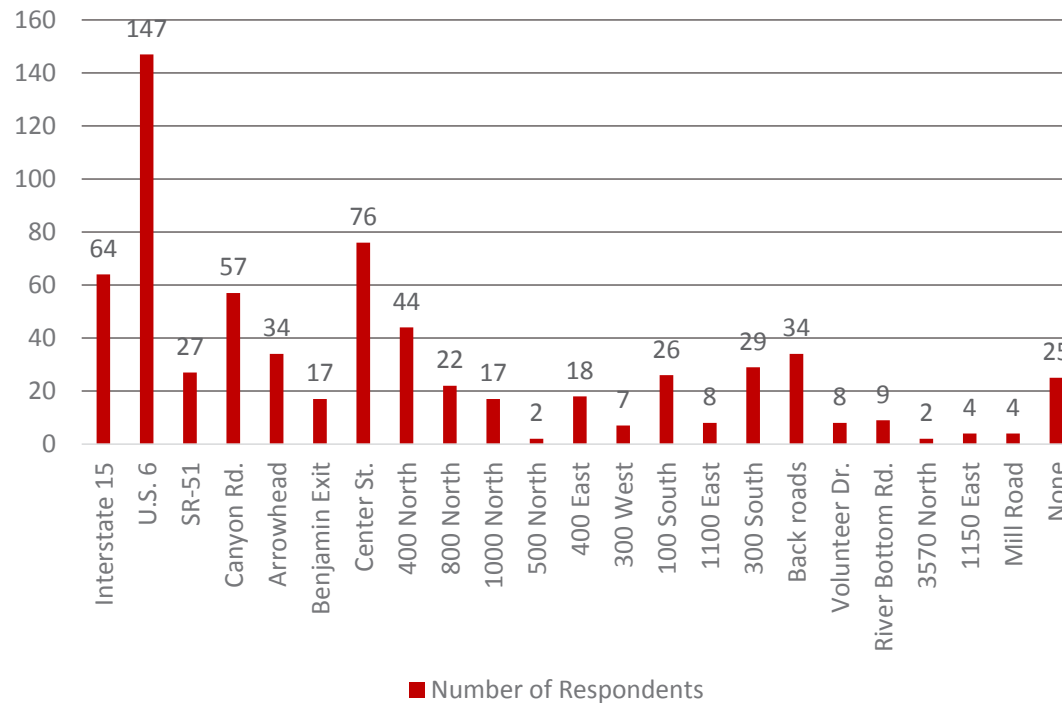




Use: Other Commuter Routes

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
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- QUESTION 10
- QUESTION 11**
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- QUESTION 18
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- QUESTION 21
- QUESTION 22
- QUESTION 23

Q11: What other routes do you use for your commute?

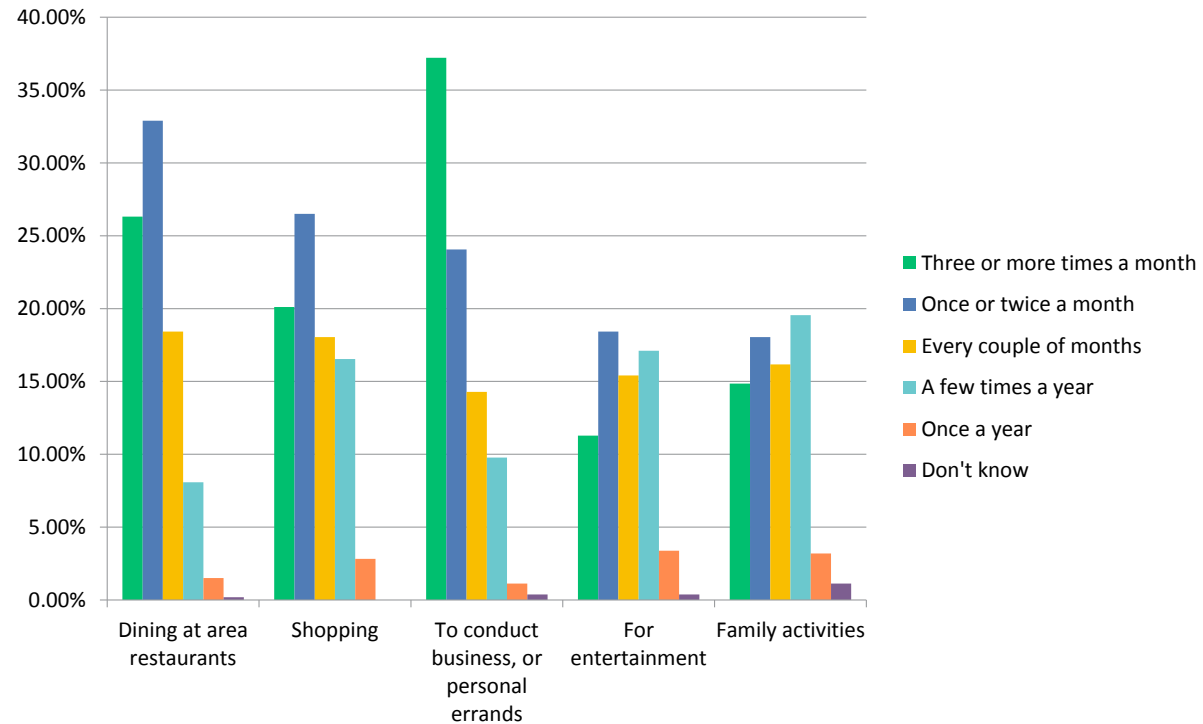




Use: Activity Frequency

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
- QUESTION 5
- QUESTION 6
- QUESTION 7
- QUESTION 8
- QUESTION 9
- QUESTION 10
- QUESTION 11
- QUESTION 12**
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- QUESTION 14
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- QUESTION 16
- QUESTION 17
- QUESTION 18
- QUESTION 19
- QUESTION 20
- QUESTION 21
- QUESTION 22
- QUESTION 23

Q12: How frequently do you or your family visit the Main Street area for the following activities?

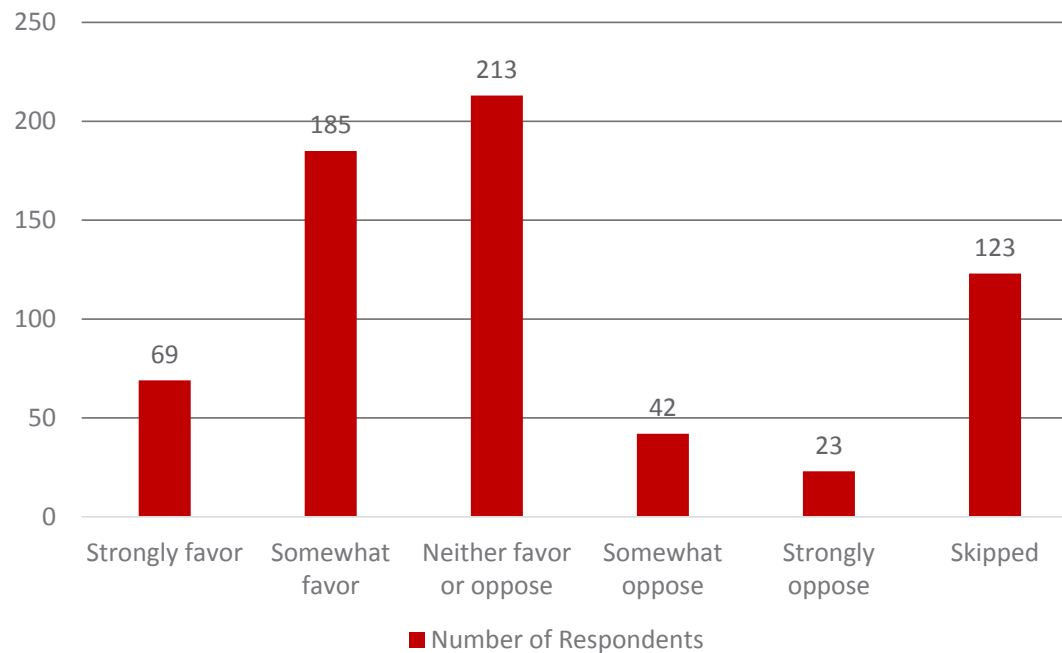




Parking: Improvements

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
- QUESTION 5
- QUESTION 6
- QUESTION 7
- QUESTION 8
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- QUESTION 10
- QUESTION 11
- QUESTION 12
- QUESTION 13**
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- QUESTION 16
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- QUESTION 18
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- QUESTION 20
- QUESTION 21
- QUESTION 22
- QUESTION 23

Q13: To improve parking on Spanish Fork Main Street, would you favor/oppose using shared parking areas that connect the Main Street businesses as well as the commercial areas to the northeast?

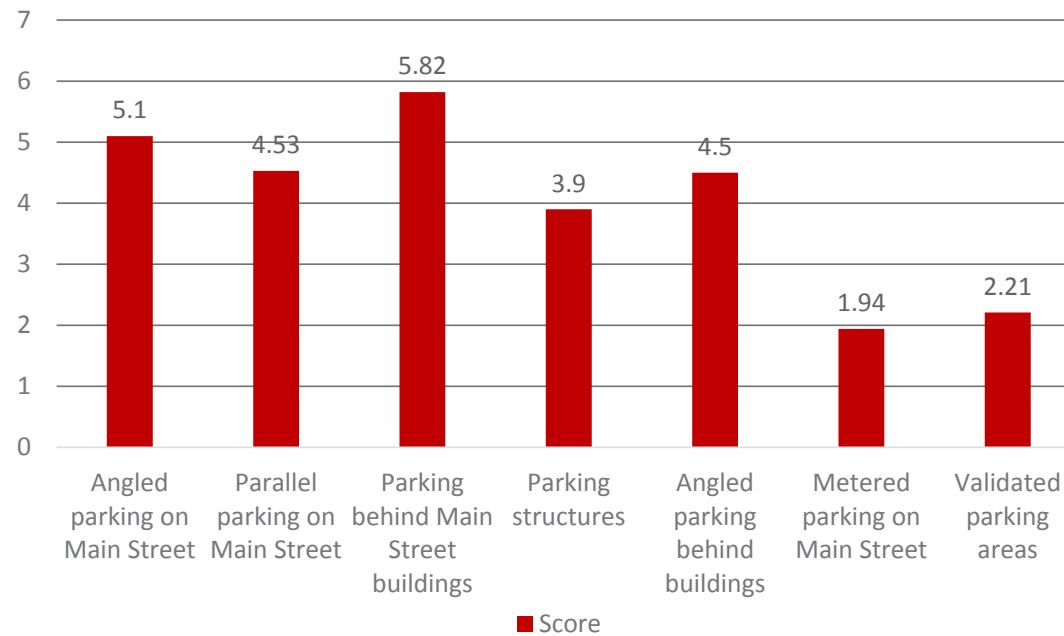




Parking: Evaluate Options

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
- QUESTION 5
- QUESTION 6
- QUESTION 7
- QUESTION 8
- QUESTION 9
- QUESTION 10
- QUESTION 11
- QUESTION 12
- QUESTION 13
- QUESTION 14**
- QUESTION 15
- QUESTION 16
- QUESTION 17
- QUESTION 18
- QUESTION 19
- QUESTION 20
- QUESTION 21
- QUESTION 22
- QUESTION 23

Q14: Spanish Fork is evaluating the available parking in the Main Street area. Please rank in order of importance (with ONE being the most important and SEVEN being the least important), what types of parking you would prefer for the surrounding area?

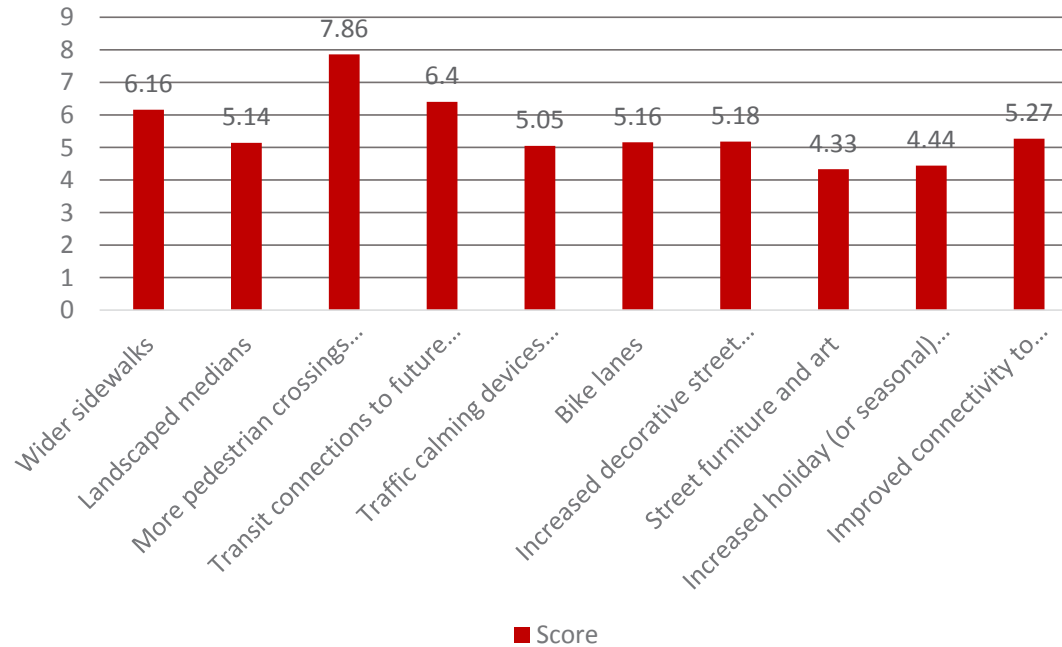




Roadway: Evaluate Options

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
- QUESTION 5
- QUESTION 6
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- QUESTION 14
- QUESTION 15**
- QUESTION 16
- QUESTION 17
- QUESTION 18
- QUESTION 19
- QUESTION 20
- QUESTION 21
- QUESTION 22
- QUESTION 23

Q15: Please rank in order of importance (with ONE being the most important and TEN being the least important), what types of roadway improvements/amenities would you like to see for Main Street?

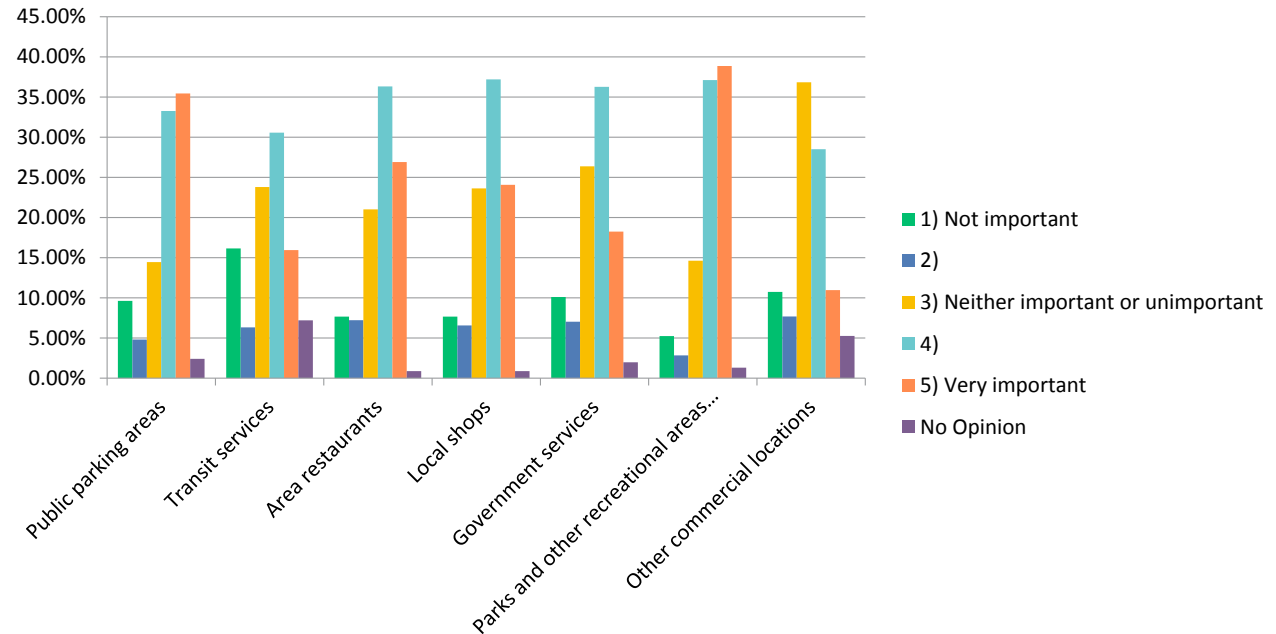




Information: Wayfinding Signs

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
- QUESTION 5
- QUESTION 6
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- QUESTION 10
- QUESTION 11
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- QUESTION 14
- QUESTION 15
- QUESTION 16**
- QUESTION 17
- QUESTION 18
- QUESTION 19
- QUESTION 20
- QUESTION 21
- QUESTION 22
- QUESTION 23

Q16: On a scale of ONE meaning “not important at all” to FIVE meaning “very important”, how important are wayfinding signs in directing you to the following locations in the Main Street area?

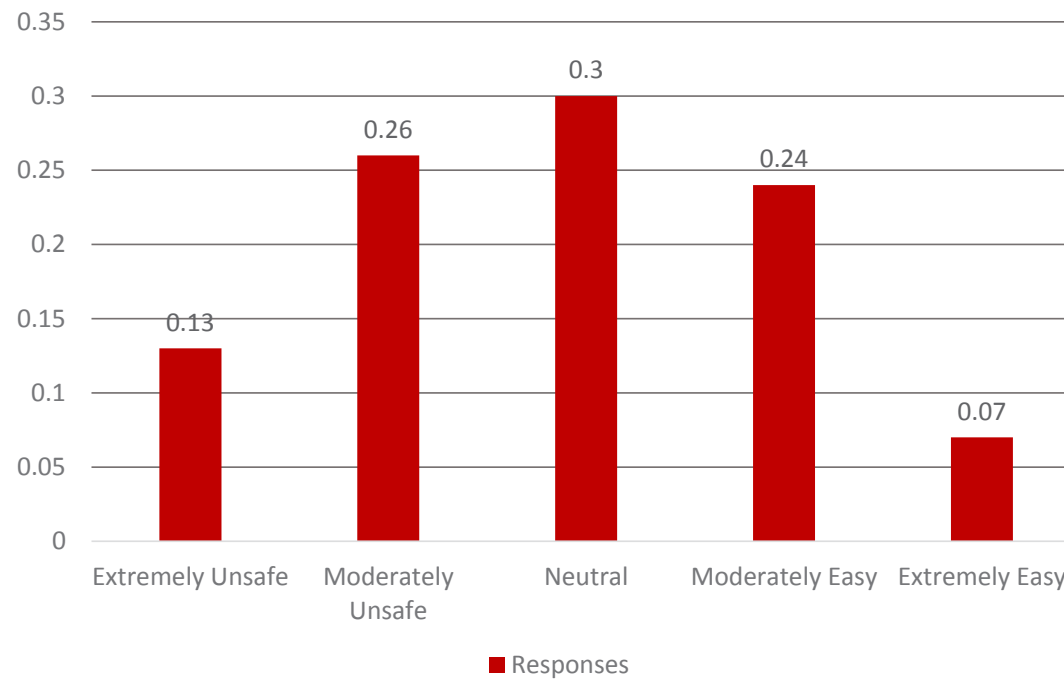




Use: Parking Safety

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
- QUESTION 5
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- QUESTION 8
- QUESTION 9
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- QUESTION 15
- QUESTION 16
- QUESTION 17**
- QUESTION 18
- QUESTION 19
- QUESTION 20
- QUESTION 21
- QUESTION 22
- QUESTION 23

Q17: On a scale of one to five with ONE meaning “extremely unsafe” and FIVE meaning “extremely easy”, please rate how safely you are able to park on Main Street:

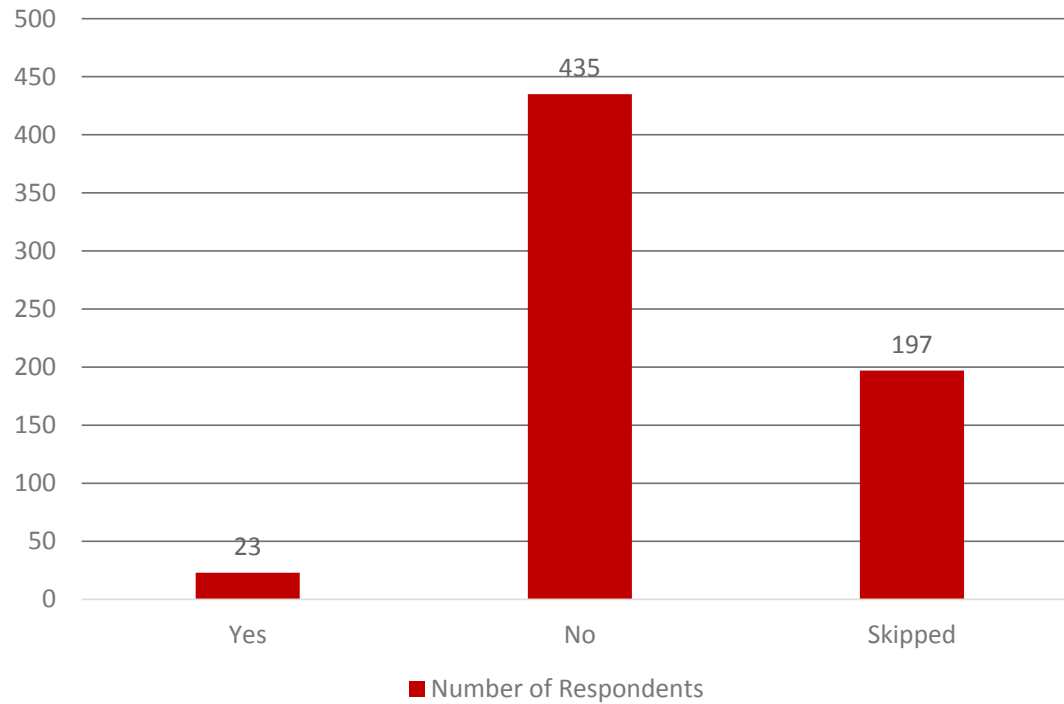




Demographics: Business Owners

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
- QUESTION 5
- QUESTION 6
- QUESTION 7
- QUESTION 8
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- QUESTION 16
- QUESTION 17
- QUESTION 18**
- QUESTION 19
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- QUESTION 21
- QUESTION 22
- QUESTION 23

Q18: Are you a business owner in the Spanish Fork Main Street area?

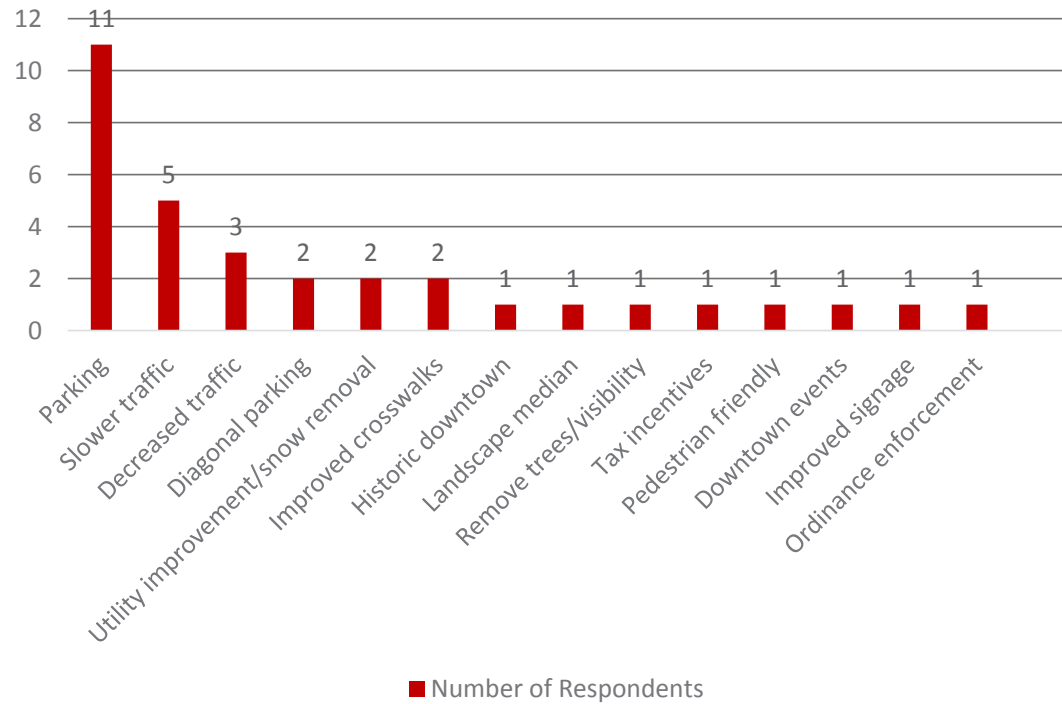




Business: Area Improvements

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
- QUESTION 5
- QUESTION 6
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- QUESTION 8
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- QUESTION 19**
- QUESTION 20
- QUESTION 21
- QUESTION 22
- QUESTION 23

Q19: What types of improvements would you like to see on Main Street to support your place of business?

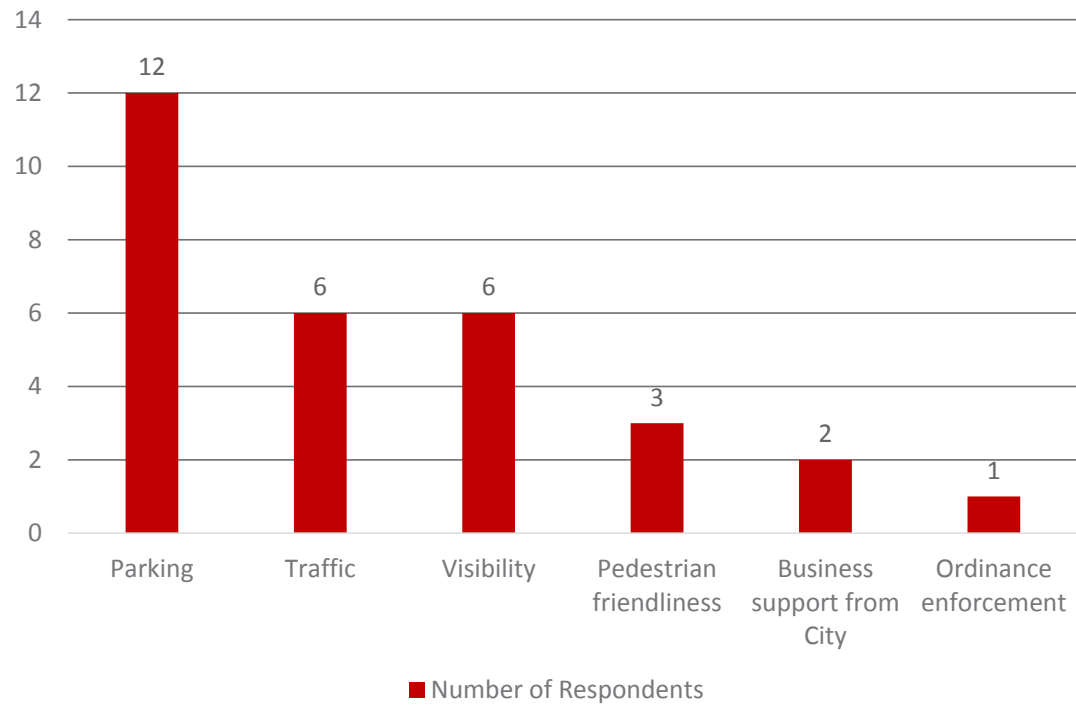




Business: Challenges

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
- QUESTION 5
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- QUESTION 8
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- QUESTION 18
- QUESTION 20**
- QUESTION 21
- QUESTION 22
- QUESTION 23

Q20: What are the greatest challenges you experience operating a business on Main Street?

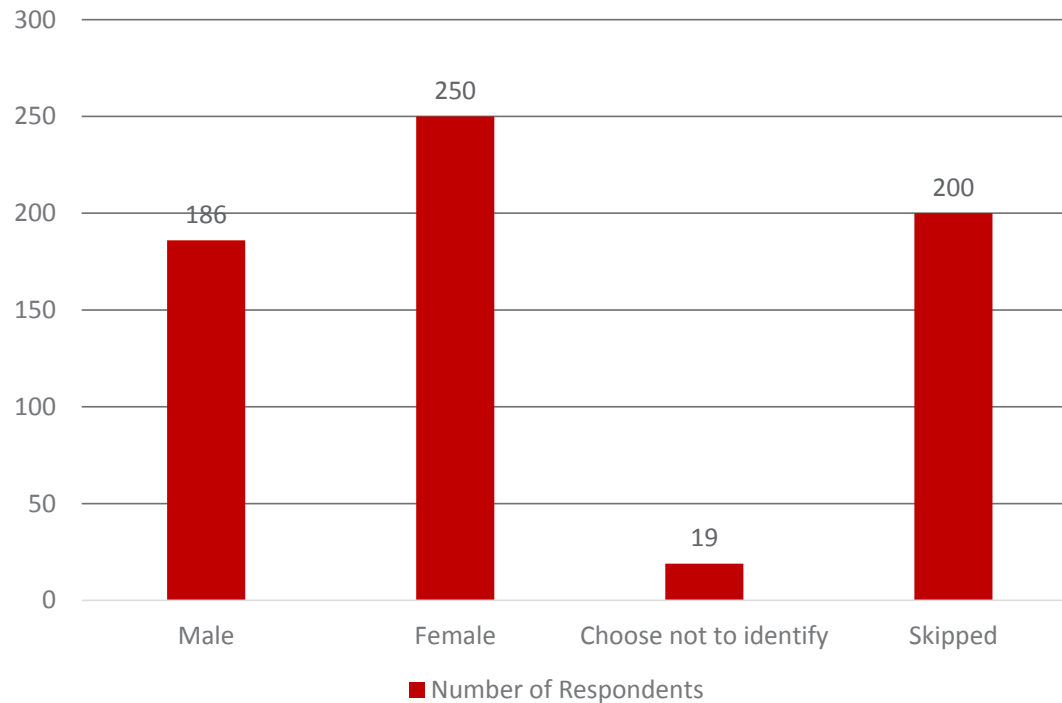




Demographics: Gender

- QUESTION 1
- QUESTION 2
- QUESTION 3
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- QUESTION 21**
- QUESTION 22
- QUESTION 23

Q21: Which gender do you most identify with?

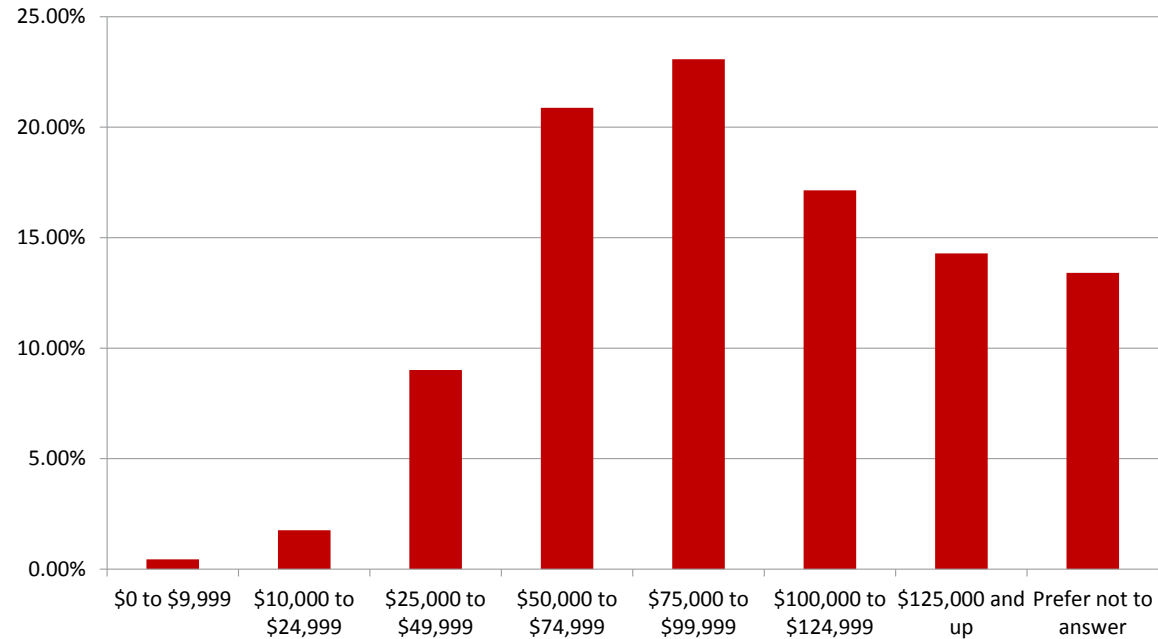




Demographics: Income

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
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- QUESTION 20
- QUESTION 22**
- QUESTION 23

Q22: How much total combined money did all members of your HOUSEHOLD earn last year?

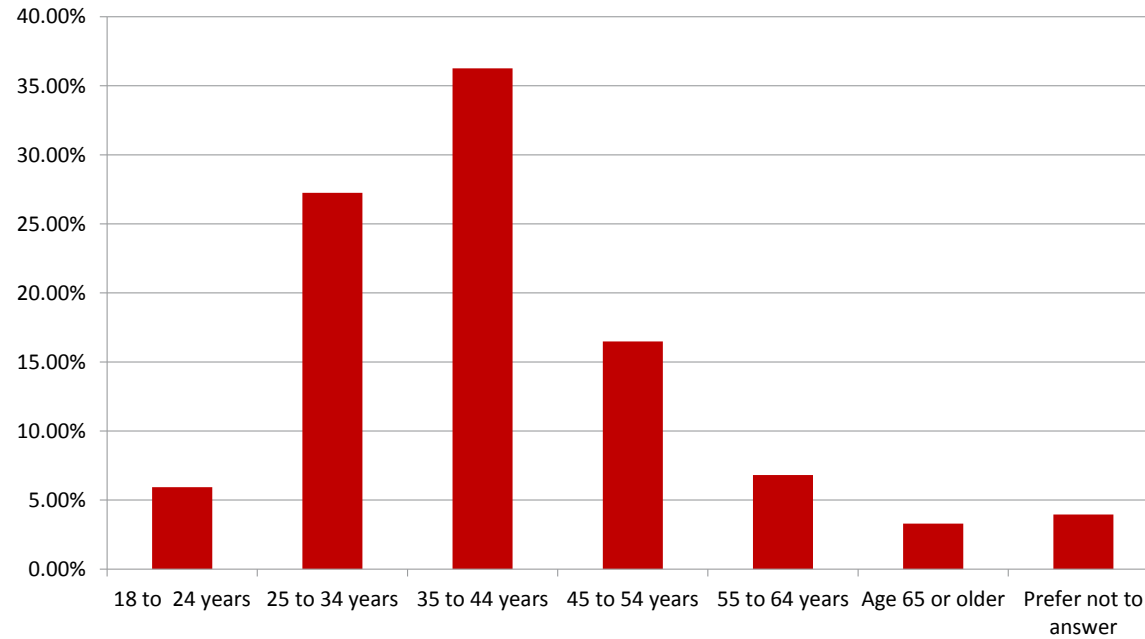




Demographics: Age

- QUESTION 1
- QUESTION 2
- QUESTION 3
- QUESTION 4
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- QUESTION 22
- QUESTION 23**

Q23: What is your age?





Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

Respondents	Response Date	Responses
1	Jul 13 2018 10:23 AM	I take Main street to I-15. It's the only way I go to work.
2	Jul 11 2018 09:38 PM	u.s. 6 is only a few blocks away, and is usually quickest for me to connect to other roads.
3	Jul 11 2018 07:13 PM	Hwy 6 and the new Canyon Creek development area
4	Jul 10 2018 09:53 AM	Frontrunner
5	Jul 09 2018 01:37 AM	Arrowhead trail and side streets to avoid tragic anf congestion on main street
6	Jul 06 2018 11:49 PM	Arrowhead Drive
7	Jul 06 2018 10:03 AM	None.
8	Jul 06 2018 09:50 AM	None, always Main St. to I-15 N
9	Jul 04 2018 06:32 PM	Sometimes Benjamin exit.
10	Jul 04 2018 06:08 PM	300 south, Center street, 400 north, 1000 north, 800 north, 900 east, 800 east
11	Jul 04 2018 12:49 PM	I go up Center street, either to access the freeway or to make my way to Springville.
12	Jul 04 2018 12:41 AM	US 6, canyon road, center street, main Street, 400 north, 800 east, 1000 north
13	Jul 04 2018 12:14 AM	I use back roads from Wal-Mart up to Provo.
14	Jul 03 2018 11:02 PM	No other options. Please put exit at center to I-15
15	Jul 03 2018 05:43 PM	Canyon Road & US 6
16	Jul 03 2018 05:12 PM	Hwy 6
17	Jul 03 2018 01:48 PM	None
18	Jul 03 2018 10:57 AM	Hwy 6 to I-15
19	Jul 02 2018 09:37 PM	500 n
20	Jul 02 2018 07:02 PM	Arrowhead Rd. To Benjamin entrance with I15
21	Jul 02 2018 05:29 PM	Canyon Rd everyday and 400 E.
22	Jul 02 2018 11:15 AM	3rd west
23	Jul 02 2018 09:45 AM	1100 east and us 6
24	Jul 02 2018 08:10 AM	Please do not destroy main Street to accommodate public transportation. This is a waste of resources. I have never seen a bus with more than. 10 people on it. Bike Lanes get used but more than. Half of the time the bikers still use the lanes intended for cars. Like it was said before there needs to be a better way for elk ridge, higland and Salem to get to there town. This would relieve a lot of traffic down main Street.



Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

25	Jul 02 2018 07:30 AM	Canyon Road. If you're going to improve traffic flow, do something about. Canyon! Especially during school pick up times, traffic is SO backed up on that road! And, speed limit signs are too few, so people go way too slow. Canyon feeds a HUGE residential chunk of SF. We need 2 lanes both ways, or at least turning lanes.
26	Jul 02 2018 03:42 AM	Canyon Road and Highway 6
27	Jul 01 2018 11:58 PM	1100 east to center street to hwy 6
28	Jul 01 2018 11:15 PM	none
29	Jul 01 2018 10:33 PM	1000 n., 800 n.
30	Jul 01 2018 09:15 PM	4th north
31	Jul 01 2018 09:09 PM	Center street
32	Jul 01 2018 09:00 PM	300 west
33	Jul 01 2018 08:18 PM	U.S. 6 is the main road used to commute.
34	Jul 01 2018 07:54 PM	U.S. 6
35	Jul 01 2018 07:54 PM	None
36	Jul 01 2018 07:05 PM	1100 East to Highway 6 to I-15, Riverbottom Road, 1100 East to Highway 51
37	Jul 01 2018 05:30 PM	300 South, Hwy 6
38	Jul 01 2018 04:10 PM	400 N and SR-51
39	Jul 01 2018 02:32 PM	Back road to Springville or Salem.
40	Jul 01 2018 02:18 PM	Backroads to springville and Salem
41	Jul 01 2018 12:05 PM	800 North
42	Jul 01 2018 11:35 AM	400 North, 1st South and Volunteer Drive
43	Jul 01 2018 10:48 AM	US-6 to I-15
44	Jul 01 2018 10:43 AM	SR
45	Jul 01 2018 10:09 AM	Highway 51
46	Jul 01 2018 10:02 AM	Arrowhead trail or 300 south
47	Jul 01 2018 09:56 AM	Center street
48	Jul 01 2018 09:42 AM	Sometimes the side streets that run parallel to main Street when it is busy
49	Jul 01 2018 09:42 AM	IFA to freeway
50	Jul 01 2018 08:52 AM	Canyon, hwy6, back roads to avoid Main St.
51	Jul 01 2018 07:10 AM	Main Street only



Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

52	Jul 01 2018 06:58 AM	Hey 6
53	Jul 01 2018 06:46 AM	Hwy 6 to I15
54	Jul 01 2018 03:17 AM	Hwy 6, Center St, Canyon Rd, 4th East, Canyon Creek area
55	Jul 01 2018 02:06 AM	From 4th north all the way passed arrowhead every day.
56	Jul 01 2018 12:52 AM	US 6
57	Jul 01 2018 12:44 AM	Highway 6 to I 15
58	Jul 01 2018 12:27 AM	I typically drive on Canyon Rd., to highway 6, to I-15
59	Jul 01 2018 12:25 AM	4th and 8th north have become primary routes for me as SF has grown
60	Jul 01 2018 12:24 AM	Highway 6 to the freeway. I use main street mostly to get to the Macey's area.
61	Jul 01 2018 12:07 AM	Hwy 6
62	Jun 30 2018 11:56 PM	800 E to Hwy 6, 400 E to 1000N to either I15 or Hwy 6
63	Jun 30 2018 11:55 PM	US 6 and the golf course road going into Salem
64	Jun 30 2018 11:43 PM	Highway 6, Canyon Road, 300 East, 1000 N
65	Jun 30 2018 11:34 PM	None
66	Jun 30 2018 11:31 PM	Highway 6.
67	Jun 30 2018 11:23 PM	Hwy 6 to the freeway
68	Jun 30 2018 11:10 PM	Side streets of Main is crowded
69	Jun 30 2018 11:02 PM	Hwy 6 to i15 north
70	Jun 30 2018 10:38 PM	Highway 6, Center Street, Canyon Road and side streets, mainly East of Main, but occasionally West of Main
71	Jun 30 2018 10:31 PM	Arrowhead trail out to the freeway.
72	Jun 30 2018 10:25 PM	Back roads to Benjamin Exit
73	Jun 30 2018 10:16 PM	I use 600 N or 800 North to get from centennial park area to I-15 or any of the businesses on the north end of town and back again. I wish the lights on Main Street had more green left turn arrows. When it gets busy it is so difficult to make a safe left turn.
74	Jun 30 2018 10:14 PM	I drive down main street for work all the way down to 3570 north.
75	Jun 30 2018 10:12 PM	Other local roads
76	Jun 30 2018 10:06 PM	Center street, I have to go on main street to get of town.
77	Jun 30 2018 10:03 PM	Hwy 6



Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

78	Jun 30 2018 09:56 PM	Almost always use Hwy 6 to get on and off 1-15 because it is so much easier especially heading north.
79	Jun 30 2018 09:44 PM	Hwy 6
80	Jun 30 2018 09:38 PM	Canyon Rd, Center Street, Hwy 6
81	Jun 30 2018 09:30 PM	I try to use Center St when I can because it's usually less congested
82	Jun 30 2018 09:27 PM	Arrowhead trail
83	Jun 30 2018 09:14 PM	hwy6
84	Jun 30 2018 09:11 PM	300 S/CanyonRd
85	Jun 30 2018 09:11 PM	Turn on center to go towards hwy 6 and then left on 400 e and then under overpass if heading to Springville. Otherwise, I use Main Street. Adding a freeway exit off Center Street would eliminate a lot of the traffic heading down Main Street, if heading for I-15.
86	Jun 30 2018 09:08 PM	Freeway
87	Jun 30 2018 09:02 PM	I connect to Highway 6 via Center Street.
88	Jun 30 2018 08:44 PM	400 North
89	Jun 30 2018 08:23 PM	I connect to I-15 SB via Mill Rd to Arrowhead, or between the fields on that road that connects to the light at the new Revere Health building. By the way, thanks for that light. It was needed.
90	Jun 30 2018 08:23 PM	Just the 6
91	Jun 30 2018 08:20 PM	Center street, 400 N
92	Jun 30 2018 08:18 PM	Center street, 400 n, 800 n, and 300 s.
93	Jun 30 2018 07:54 PM	back roads, they aren't as crowded
94	Jun 30 2018 07:50 PM	From nearby street to US 6 and to I-15
95	Jun 30 2018 07:35 PM	4th north, springville highway
96	Jun 30 2018 07:33 PM	Hwy 6, 800 N, Canyon Rd, Arrowhead trail, river bottoms.
97	Jun 30 2018 07:31 PM	I don't commute.
98	Jun 30 2018 07:08 PM	US-6, 1000N
99	Jun 30 2018 06:54 PM	300 South, 1150 East, Hwy 6
100	Jun 30 2018 06:35 PM	Old highway between Springville and Spanish fork. Center St.
101	Jun 30 2018 06:33 PM	Canyon Road and Highway 6.
102	Jun 30 2018 06:26 PM	Highway 6
103	Jun 30 2018 06:25 PM	300 W



Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

104	Jun 30 2018 06:25 PM	Just main street to I 15
105	Jun 30 2018 06:02 PM	400 North, highway 51, 800 east, highway 6
106	Jun 30 2018 05:58 PM	Hwy 6
107	Jun 30 2018 05:55 PM	400 north
108	Jun 30 2018 05:54 PM	800 North, 400 North and 1000 North
109	Jun 30 2018 05:47 PM	I work and live in Spanish Fork. I usually avoid Main Street if I can.
110	Jun 30 2018 05:39 PM	I do not want a Center Street Exit if that's why your asking.
111	Jun 30 2018 05:30 PM	Old highways, 100 east, 100 west, to get through town When they redone the freeway interchange they screwed things up by making you go to main street from us 6 to get to north main instead of by passing until the south I 15 entrance to freeway
112	Jun 30 2018 05:28 PM	Center Street to US 6, 500 East to 1000 North to US 6.
113	Jun 30 2018 05:22 PM	Highway 6, Center street, 400 n
114	Jun 30 2018 05:19 PM	U.S. 6
115	Jun 30 2018 05:18 PM	Up 800 North to expressway lane, then on to Springville on the old highway
116	Jun 30 2018 05:12 PM	400 north, 1000 north
117	Jun 30 2018 05:11 PM	Arrowhead to Benjamin exit/entrance of i15
118	Jun 30 2018 05:10 PM	I Sometimes take the I-15 Benjamin exit to get to and from my house. I would love an I-15 exit/entrance at Center street.
119	Jun 30 2018 04:58 PM	Route 6
120	Jun 30 2018 04:55 PM	800 n
121	Jun 30 2018 04:54 PM	Us6
122	Jun 30 2018 04:41 PM	Benjamin exit
123	Jun 30 2018 04:31 PM	State highway through Salem.
124	Jun 30 2018 04:31 PM	800 N, Center St, 200 S
125	Jun 30 2018 04:30 PM	None
126	Jun 30 2018 04:24 PM	Highway 6
127	Jun 30 2018 04:23 PM	Highway 6 and Canyon Roads crossing at 1100 or 2250 E
128	Jun 30 2018 04:20 PM	800E to hwy6 and occasionally center St to hwy6



Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

129	Jun 30 2018 04:16 PM	My own car
130	Jun 30 2018 04:08 PM	100 West
131	Jun 30 2018 03:58 PM	US 6 and Canyon Road, since I live on the East side
132	Jun 30 2018 03:58 PM	Not sure how to answer this.
133	Jun 30 2018 03:55 PM	Center street and highway 6
134	Jun 30 2018 03:51 PM	Highway 6
135	Jun 30 2018 03:49 PM	None
136	Jun 30 2018 03:48 PM	Hwy 6, 400 N
137	Jun 30 2018 03:44 PM	Hwy 6
138	Jun 30 2018 03:28 PM	US-6, Canyon Road, 800 east
139	Jun 30 2018 03:22 PM	400 North to I-15, 2550 E to Hwy 6, 400 North to Mapleton Hwy 89, 400 North down to 4 way stop and North to Springville. Occasionally down to Main Street and then onto I-15.
140	Jun 30 2018 03:11 PM	Main Street
141	Jun 30 2018 03:11 PM	US 6
142	Jun 30 2018 03:02 PM	300 south/Canyon road, Arrowhead trail, Volunteer drive, West park drive
143	Jun 30 2018 03:00 PM	3rd south, 8th north, canyon Rd, rt 6
144	Jun 30 2018 02:58 PM	Highway 6
145	Jun 30 2018 02:57 PM	Center Street and US 6
146	Jun 30 2018 02:53 PM	u.S 6 to get to I-15
147	Jun 30 2018 02:52 PM	I-15
148	Jun 30 2018 02:50 PM	100 S
149	Jun 30 2018 02:45 PM	I 15
150	Jun 30 2018 02:43 PM	occasionally i use 300 s or center street.
151	Jun 30 2018 02:41 PM	Top of center to hiway 6
152	Jun 30 2018 02:40 PM	river bottoms road to main street and to the rodeo
153	Jun 30 2018 02:40 PM	300 South
154	Jun 30 2018 02:34 PM	Center Street, 300 South/Canyon Road
155	Jun 30 2018 02:31 PM	400 n
156	Jun 30 2018 02:22 PM	Highway 6, canyon road, i-15
157	Jun 30 2018 02:18 PM	Highway 6, center street, I-15



Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

158	Jun 30 2018 02:17 PM	1st South and 4th South to Leland/Benjamin area
159	Jun 30 2018 02:14 PM	800 East
160	Jun 30 2018 02:11 PM	Highway 6
161	Jun 30 2018 02:10 PM	Back roads to Mapleton, Salem, and Springville.
162	Jun 30 2018 02:06 PM	Canyon Road and Highway six.
163	Jun 30 2018 02:05 PM	Highway 6, 400 north
164	Jun 30 2018 02:01 PM	US 6
165	Jun 30 2018 01:59 PM	300 South, 4th North, 800 North
166	Jun 30 2018 01:58 PM	Arrowhead trail
167	Jun 30 2018 01:56 PM	6
168	Jun 30 2018 01:55 PM	400 n, 300 e (Why don't these roads utilize more roundabouts?)
169	Jun 30 2018 01:55 PM	Center to us 6 down to i15 and over to main
170	Jun 30 2018 01:53 PM	Hwy 6 and 400n
171	Jun 30 2018 01:53 PM	Usually US 6 to I-15
172	Jun 30 2018 01:52 PM	I take 1st south out to Benjamin every day.
173	Jun 30 2018 01:52 PM	Canyon road
174	Jun 30 2018 01:50 PM	Arrowhead trail to i-15, Highway 89 to Salem.
175	Jun 30 2018 01:49 PM	400 N
176	Jun 30 2018 01:48 PM	I15 from arrowhead to main street
177	Jun 30 2018 01:46 PM	None
178	Jun 30 2018 01:45 PM	Canyon road to I-15 on arrowhead. Also 9th east to hwy 6 to I-15
179	Jun 30 2018 01:42 PM	Highway 6
180	Jun 30 2018 01:38 PM	Canyon Road, Center street
181	Jun 30 2018 01:33 PM	300 south and then back roads if it's a busy time of day.
182	Jun 30 2018 01:31 PM	US 6 to i15
183	Jun 30 2018 01:26 PM	Canyon Rd/300 s
184	Jun 30 2018 01:24 PM	Old highway
185	Jun 30 2018 01:23 PM	Canyon Rd to 300 S./Main and 2550 E and Hwy 6 mostly since I live on the east side.
186	Jun 30 2018 01:23 PM	3rd and 4th east
187	Jun 30 2018 01:20 PM	Canyon road 4th north 8th north



Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

188	Jun 30 2018 01:17 PM	400 north
189	Jun 30 2018 01:17 PM	None
190	Jun 30 2018 01:13 PM	I use a lot of side roads to avoid congestion on main, center, 400 N, 1000 N, 600 E, and 800 E. These roads have gotten to the point that unless I have to use them, I avoid them at all costs. Im pretty sure im not the only one.
191	Jun 30 2018 01:11 PM	Any road to avoid mainstreet. With the fewest stop signs.
192	Jun 30 2018 01:10 PM	Center Street to hwy 6 to I-15 northbound and vise versa
193	Jun 30 2018 01:06 PM	State Rd thru Mapleton and springville
194	Jun 30 2018 01:06 PM	River bottoms road
195	Jun 28 2018 11:23 PM	Center Street, 500 east, us 6, i15
196	Jun 27 2018 04:10 PM	There are not any other options. I have to take main street if I don't want to weave through neighborhoods.
197	Jun 25 2018 10:18 PM	Canyon road/300 south
198	Jun 25 2018 08:54 PM	400 north
199	Jun 25 2018 02:20 PM	Canyon road/300south
200	Jun 25 2018 12:14 PM	use main street, us 6 and I15, 1000 North and also Arrowhead Trail
201	Jun 25 2018 11:08 AM	None
202	Jun 22 2018 08:10 AM	I-15, Arrowhead Trail
203	Jun 21 2018 06:15 PM	US 6, Expressway lane
204	Jun 21 2018 05:41 PM	Highway 6
205	Jun 21 2018 04:24 PM	back roads
206	Jun 21 2018 03:22 PM	Main Street
207	Jun 21 2018 09:52 AM	Center Street, Canyon Road, 800 East
208	Jun 21 2018 06:42 AM	US 6 to i15

Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

209	Jun 20 2018 08:59 PM	I live over by the cemetery. I use US-6 usually because it is normally faster for me to get home. I can then get off over by Cafe Rio/Vasa Fitness area, or travel down center street to travel to where I live. If I'm going to Macey's I usually take side streets. If I'm going over by Walmart/Cinemark area I usually go to the US-6. Main street usually has a lot of cars driving down it in the late afternoon/night time, plus all the traffic lights, which makes it a hassle to travel down. If you make it more like a local road instead of a commuter road, and you don't fix the traffic issue, then you're going to have issues on your hands with traffic. Definitely don't make it like Provo Center street. Their traffic flow and weird parking makes me rarely ever want to go there. If I do go down main street to get to I-15 it will be normally on the weekends. Going down main street vs heading over to US-6 takes about the same time for me in those circumstances.
210	Jun 20 2018 07:25 PM	Highway 6 to I-15
211	Jun 20 2018 11:18 AM	Highway 6 and I-15 north of the city
212	Jun 20 2018 02:38 AM	Depends
213	Jun 19 2018 10:37 PM	I use 3rd east to avoid all the stop lights and the people who only drive in the right lane north bound on main. The worst time to travel in SF is 2:15 to 3:45pm m-f during the school year. The schools cut the city in half and you can't get past center street due to the pedestrians, high school kids and carpool chaos.
214	Jun 19 2018 09:00 PM	Very rarely, but sometimes I use take Arrowhead Trail west to get on the 8000 S i-15 entrance.
215	Jun 19 2018 07:22 PM	Old hwy to Springville, arrowhead, north main.
216	Jun 19 2018 05:16 PM	Highway 6
217	Jun 19 2018 10:08 AM	400 North, 100 South, 400 East (much less congested than Main street when traveling north and south.)
218	Jun 19 2018 09:11 AM	Hwy 6 and canyon
219	Jun 19 2018 08:35 AM	Highway 6, Canyon Road, Riverbottoms Road, Powerhouse
220	Jun 18 2018 11:07 PM	US-6
221	Jun 18 2018 10:44 PM	Expressway Lane
222	Jun 18 2018 10:01 PM	Us 6 and I 15
223	Jun 18 2018 09:36 PM	None
224	Jun 18 2018 07:25 PM	None
225	Jun 18 2018 06:08 PM	1st south. and center st
226	Jun 18 2018 05:50 PM	Canyon Rd. 300 S. Arrowhead Trail
227	Jun 18 2018 04:47 PM	State highway to Salem and Payson and Springville. North Main 1400 to Springville.



Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

228	Jun 18 2018 04:39 PM	hwy 6
229	Jun 18 2018 04:15 PM	Hwy 6
230	Jun 18 2018 02:21 PM	arrowhead trail to the freeway.
231	Jun 18 2018 01:54 PM	Highway 6, Canyon Road and Powerhouse Road.
232	Jun 18 2018 01:47 PM	from main street to arrow head trail getting on I-15 s at Benjamin or from 800 e Spanish fort to on route 50 to get to Springville main street
233	Jun 18 2018 01:41 PM	Benjamin exit to avoid main street
234	Jun 18 2018 01:32 PM	1100 East to Hwy 6 and springville
235	Jun 18 2018 01:28 PM	different depends on time of day
236	Jun 18 2018 01:27 PM	Highway 198
237	Jun 18 2018 12:51 PM	Highway 6 and Canyon Road
238	Jun 18 2018 12:47 PM	I-15 to north Utah county.
239	Jun 18 2018 12:41 PM	400 North to main street sometimes or sometimes I go down center street to main street then head north
240	Jun 18 2018 12:39 PM	Highway 6 as well as 300 south and center street
241	Jun 18 2018 12:27 PM	None
242	Jun 18 2018 12:21 PM	Straight Line to Springville
243	Jun 18 2018 12:17 PM	400 East Hwy 6
244	Jun 18 2018 12:16 PM	Arrowhead trail connecting in the on ramp for I-15
245	Jun 18 2018 12:05 PM	HWY 6
246	Jun 18 2018 10:15 AM	Sometime drive in roads parallel to Main Street to avoid congestion
247	Jun 18 2018 08:59 AM	I use 4th east to 1000 north
248	Jun 18 2018 08:35 AM	400 N
249	Jun 18 2018 08:07 AM	300 South and/or 300 East
250	Jun 18 2018 07:37 AM	None...
251	Jun 17 2018 09:52 PM	Highway 89 from mapleton into Springville, the old Springville highway from 400 N in SF to Springville main St
252	Jun 17 2018 09:03 PM	Canyon Rd to Hwy 6 to I-15 only
253	Jun 17 2018 08:53 PM	Center street, canyon road and arrowhead trail.



Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

254	Jun 17 2018 08:43 PM	SR6 to freeway
255	Jun 17 2018 08:26 PM	Hwy 6
256	Jun 17 2018 05:15 PM	Highway 6
257	Jun 17 2018 04:16 PM	Canyon road, 1100 E, Center street, hwy 6
258	Jun 17 2018 03:42 PM	6 East to the Texaco/Gold's Gym to the freeway going north, or 6 East to 1000 North and enter freeway going north
259	Jun 17 2018 06:43 AM	2550 East to Highway 6.
260	Jun 17 2018 01:32 AM	Hwy 6
261	Jun 16 2018 11:16 PM	400 south
262	Jun 16 2018 11:12 PM	No comment
263	Jun 16 2018 10:49 PM	When Main Street is busy or school is in session, I will go to the 8000 South I15 on ramp to head North instead of taking Main Street. Even though the distance is further, Main Street gets backed up when I leave for work in the morning.
264	Jun 16 2018 09:46 PM	Mill road, Volunteer way, I15
265	Jun 16 2018 04:39 PM	None
266	Jun 16 2018 03:58 PM	Hwy 6, Canyon road, 4th north, hwy 89
267	Jun 16 2018 02:14 PM	Canyon road , 8th east, 4th east, Volunteer drive, 4th north
268	Jun 16 2018 01:54 PM	I also use third West, going north to 8th and the freeway when I need to go north, and I also use 6th west, going by the ball fields to Main Street when going south.
269	Jun 16 2018 01:30 PM	Center street, 400 north from Maple Mtn to Lake shore, Hwy 51 to Springville, 800 East, 800 north, 1000 North. These are daily routes multiple times a day
270	Jun 16 2018 01:27 PM	100 s to Main to I15, Otherwise through Benjamin... Long detour but worth it to avoid traffic
271	Jun 16 2018 12:38 PM	None
272	Jun 16 2018 11:40 AM	Center Street. 300 East.
273	Jun 16 2018 11:13 AM	I have been using 200 East to avoid the congestion on Main St.
274	Jun 16 2018 10:10 AM	Arrowhead trail to freeway. Or canyon road for east side of Spanish Fork
275	Jun 16 2018 09:58 AM	4th N
276	Jun 16 2018 09:51 AM	Arrowhead, 1st south around pallet factory to freeway
277	Jun 16 2018 09:37 AM	I use as many side and back roads as possible. I hate Main Street.



Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

278	Jun 16 2018 09:23 AM	800 East to US-6, Center Street to US-6
279	Jun 16 2018 07:44 AM	Highway 6, Canyon road
280	Jun 16 2018 07:37 AM	I 15, the old highway
281	Jun 16 2018 07:10 AM	I-15
282	Jun 16 2018 06:55 AM	Highway 6, Center street, 400 N, 1000 N
283	Jun 16 2018 06:11 AM	Center Street, 8th East, 1-15, sr-152
284	Jun 16 2018 05:51 AM	I live near Mill road. Arrowhead trail to I-15 is my primary route- even for going to Costco sometimes as Main Street traffic is ridiculous during rush hour.
285	Jun 16 2018 04:33 AM	Highway 6
286	Jun 16 2018 03:55 AM	Powerhouse Road, Canyon Road, River Bottoms when/if they ever finish laying pipe.
287	Jun 16 2018 12:53 AM	Highway 6
288	Jun 16 2018 12:16 AM	Highway 6, Canyon Road, 2550 E, River Bottoms Rd, 1000 N. Center Street, Canyon Creek Parkway, 1150 (?) East (the connector street from Canyon Road to Center, passes Larson Elementary), Volunteer Drive, 7200 S.
289	Jun 16 2018 12:15 AM	300 West, 400 East, 800 East, Volunteer
290	Jun 16 2018 12:12 AM	Hwy 6, Canyon Road, Center Street
291	Jun 16 2018 12:05 AM	US6
292	Jun 15 2018 11:36 PM	Hwy 6
293	Jun 15 2018 11:35 PM	None
294	Jun 15 2018 11:02 PM	When I drop my son off from school I'll go down to the Benjamin exit and get on 1-15 in south Spanish Fork.
295	Jun 15 2018 10:44 PM	US6
296	Jun 15 2018 10:43 PM	400 south
297	Jun 15 2018 10:42 PM	None
298	Jun 15 2018 10:33 PM	Arrowhead trail all the way down to the Benjamin freeway access.
299	Jun 15 2018 10:27 PM	Hwy 6
300	Jun 15 2018 10:18 PM	Center street, 4th north, 4th East
301	Jun 15 2018 10:17 PM	Hey 6
302	Jun 15 2018 10:11 PM	SR-15



Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

303	Jun 15 2018 10:08 PM	Main is our main commute.
304	Jun 15 2018 09:44 PM	NA
305	Jun 15 2018 09:26 PM	Us 6, hwy 89, I 15
306	Jun 15 2018 09:15 PM	Arrowhead trail to the 8000 S freeway entrance
307	Jun 15 2018 08:49 PM	400 North to Main Street
308	Jun 15 2018 08:46 PM	Highway 6, Center Street
309	Jun 15 2018 08:45 PM	Highway 6
310	Jun 15 2018 08:44 PM	Center Street. US 6
311	Jun 15 2018 08:40 PM	US 6 and 1000 N
312	Jun 15 2018 08:38 PM	Volunteer drive behind high school and back roads to the north end of Main Street to get to freeway
313	Jun 15 2018 08:34 PM	Center street
314	Jun 15 2018 08:30 PM	Center to us-6 to get on 15
315	Jun 15 2018 08:29 PM	1100 east to us 6 to I 15
316	Jun 15 2018 08:09 PM	Main Street from Center Street to 3200 N and Main Street to Kirby Lane
317	Jun 15 2018 07:55 PM	Hwy 6, state road 51 to get to Springville.
318	Jun 15 2018 07:53 PM	Canyon road, 2550 E, Hwy 6. Occasionally Hwy 89 to springville.
319	Jun 15 2018 07:50 PM	400 N.
320	Jun 15 2018 07:41 PM	US6
321	Jun 15 2018 07:40 PM	I work from home
322	Jun 15 2018 07:36 PM	Highway 6
323	Jun 15 2018 07:31 PM	Center street, 400 North and 800 North
324	Jun 15 2018 07:27 PM	Hwy 6, Center Street, Canyon Rd, and Arrow Head Trail
325	Jun 15 2018 07:19 PM	Main Street and 3rd South
326	Jun 15 2018 07:01 PM	Mainly Spanish Fork Main Street, on occasion the Benjamin/Salem I-15 access
327	Jun 15 2018 06:53 PM	SR51
328	Jun 15 2018 06:19 PM	Hwy 6, I15, Canyon Rd
329	Jun 15 2018 06:10 PM	Canyon Road
330	Jun 15 2018 06:05 PM	Hwy 6 and Canyon Road
331	Jun 15 2018 06:02 PM	400 North



Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

332	Jun 15 2018 05:56 PM	I avoid main street at all costs. I feel the speed limit is way too slow and for some unknown reason, people only travel on the right lane making traffic backed up way too far and it makes taking right turns impossible unless you want to wait 30 minutes just to travel a few blocks.
333	Jun 15 2018 05:56 PM	Canyon Road, Arrowhead Trail, 800 E.
334	Jun 15 2018 05:55 PM	Highway 6, 400 North, I-15
335	Jun 15 2018 05:55 PM	Route 6 to I15
336	Jun 15 2018 05:49 PM	Center Street, 800 East
337	Jun 15 2018 05:34 PM	Top of Center Street and 8th east.
338	Jun 15 2018 05:18 PM	Highway 6
339	Jun 15 2018 05:15 PM	Don't commute, I am retired.
340	Jun 15 2018 05:11 PM	The newer options aren't really faster. Speeds are higher and longer distances keep me from using them regularly. When getting around/through town Main Street is the best option.
341	Jun 15 2018 05:05 PM	Canyon Road, US 6
342	Jun 15 2018 05:00 PM	I-15, US 89, Hwy 52
343	Jun 15 2018 04:56 PM	None others
344	Jun 15 2018 04:55 PM	Main all the way to Springville
345	Jun 15 2018 04:55 PM	Center Street to Highway 6 to I15. The traffic is usually awful on main street going toward I15
346	Jun 15 2018 04:54 PM	Arrowhead trail, Main Street, and center street
347	Jun 15 2018 04:52 PM	Center Street
348	Jun 15 2018 04:38 PM	None
349	Jun 15 2018 04:35 PM	Backroads
350	Jun 15 2018 04:33 PM	SR 51
351	Jun 15 2018 04:25 PM	Hwy 6 to I15
352	Jun 15 2018 04:20 PM	400 South and Center Street
353	Jun 15 2018 04:14 PM	Arrowhead to the Benjamin exit to avoid traffic. I'll also use side streets when Main Street is a parking lot.
354	Jun 15 2018 04:14 PM	Canyon, Hwy 6, 400 E, 800 E
355	Jun 15 2018 04:10 PM	I 15
356	Jun 15 2018 04:08 PM	Highway 6



Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

357	Jun 15 2018 04:03 PM	Hwy 6, center street, 4th north.
358	Jun 15 2018 04:00 PM	US 6
359	Jun 15 2018 03:57 PM	Hwy 6 to I15, Powerhouse road to Hwy 89 in Salem, Hwy 6 to Hwy 89 in Mapleton
360	Jun 15 2018 03:53 PM	Center st
361	Jun 15 2018 03:50 PM	800 east to vasa hwy 6
362	Jun 15 2018 03:49 PM	None
363	Jun 15 2018 03:41 PM	Benjamin exit or 400 n to springville
364	Jun 15 2018 03:39 PM	Main Street (from the city library going north to I-15 entrance
365	Jun 15 2018 03:34 PM	I only use main street since there isn't a center street exit.
366	Jun 15 2018 03:31 PM	Canyon Road, Scenic Drive, 1100 East, Center Street east end, Rt 6.
367	Jun 15 2018 03:29 PM	I almost always use Main St. When I don't, though, I use back roads from my house to Arrowhead to get on the freeway going southbound.
368	Jun 15 2018 03:26 PM	State road 64
369	Jun 15 2018 03:10 PM	Highway 6 the most, but even that route is getting extremely congested now with all the Canyon Creek developments going on
370	Jun 15 2018 03:04 PM	Canyon Road and US 6
371	Jun 15 2018 02:53 PM	U S 6, 800 east, 400 east
372	Jun 15 2018 02:44 PM	400 north, Center street, 300 south
373	Jun 15 2018 02:43 PM	I live close to highway 6, so I frequently take that and get onto i-15
374	Jun 15 2018 02:42 PM	Center Street
375	Jun 15 2018 02:40 PM	HWY 6, 400N, 800N, HWY 51, Expressway Ln.
376	Jun 15 2018 02:40 PM	U.S 6
377	Jun 15 2018 02:37 PM	200 W, 400 N, Center St
378	Jun 15 2018 02:37 PM	400 North (SR 51)
379	Jun 15 2018 02:35 PM	I have used 300 west
380	Jun 15 2018 02:32 PM	Main street to I-15 ,Main Street to Springville
381	Jun 15 2018 02:32 PM	Highway 6 to I-15
382	Jun 15 2018 02:29 PM	Back roads to Springville
383	Jun 15 2018 02:29 PM	Highway 6 and canyon road. If any road needs to be improved it should be canyon road.
384	Jun 15 2018 02:25 PM	Bottoms Road



Question 11 Responses

Q 11: WHAT OTHER ROUTES DO YOU USE FOR YOUR COMMUTE?

385	Jun 15 2018 02:21 PM	Arrowhead Trail to I-15
386	Jun 15 2018 02:20 PM	All North/South roads between 3rd E. & 6th E. Center St 3rd S. & 9th N.
387	Jun 15 2018 02:16 PM	300 South/Canyon Road
388	Jun 15 2018 02:11 PM	2550 E to Hwy 6
389	Jun 15 2018 02:09 PM	Nope
390	Jun 15 2018 02:05 PM	300 south to main. Or center street to main. depends on destination. I try to avoid main to go North/South except to the sports park
391	Jun 15 2018 02:02 PM	I-15 to highway 6
392	Jun 15 2018 02:01 PM	300 west or center street
393	Jun 15 2018 01:57 PM	I use Highway 6, mostly, unless there is an accident or construction, then I use Main
394	Jun 15 2018 01:57 PM	Highway 51, highway 6 to I 15
395	Jun 15 2018 01:56 PM	800 N, 1000 N and 300 S
396	Jun 15 2018 01:52 PM	Only main st to I-15. I wish there was an exit at center street so I don't have to get in the grid lock on main street to access the freeway
397	Jun 15 2018 01:47 PM	300 N by the elementary school to get to Springville
398	Jun 15 2018 01:44 PM	Center street and Canyon road/300 South.
399	Jun 15 2018 01:43 PM	US 6
400	Jun 15 2018 01:43 PM	Riverbottom Road, Highway 89
401	Jun 15 2018 01:40 PM	Back roads
402	Jun 15 2018 01:38 PM	Bicycle
403	Jun 15 2018 01:37 PM	Canyon Road. 2550 E. 1150 E. US 6.
404	Jun 15 2018 01:34 PM	4th North and 800 East



Question 19 Responses

Q 19: WHAT TYPES OF IMPROVEMENTS WOULD YOU LIKE TO SEE ON MAIN STREET TO SUPPORT YOUR PLACE OF BUSINESS?

Respondents	Response Date	Responses
1	Jun 30 2018 04:48 PM	Unsure
2	Jun 30 2018 04:21 PM	Better parking options
3	Jun 30 2018 02:57 PM	Parking, slower traffic, less traffic
4	Jun 25 2018 11:13 AM	Parking
5	Jun 22 2018 08:13 AM	Slower Speed limit
6	Jun 21 2018 03:29 PM	Diagonal parking (for sure this would help)
7	Jun 19 2018 07:42 PM	Historic downtown district sign, diagonal parking from 400 N. to 100 S. Small artistic island in the middle of Main Street with sparse trees. Trees removed from sidewalks blocking Store fronts. Speed limit decreased and enforced in historic district.
8	Jun 19 2018 06:54 AM	More accomidations for customer parking.
9	Jun 18 2018 10:56 PM	Matching funds or tax incentives to building improvements, added parking. Cooperation and city assistance in relocating outdated utility poles and utilities.
10	Jun 18 2018 07:29 PM	Crosswalks!!!!
11	Jun 18 2018 06:04 PM	Better traffic flow
12	Jun 18 2018 04:43 PM	enforcement of timed parking, sidewalk snow removal
13	Jun 18 2018 01:41 PM	parking friendly options
14	Jun 18 2018 01:40 PM	Slower, quieter traffic. More pedestrian friendly. More events downtown-not at the park!- on the sidewalks, in front of businesses, etc. Signs and promotions for downtown. Don't just spend all the city money and time on the hwy 6 developments.
15	Jun 18 2018 01:26 PM	More parking and slowing the speed limit, improved crosswalk options
16	Jun 18 2018 01:00 PM	More parking
17	Jun 18 2018 12:24 PM	Parking Solutions on Main Street
18	Jun 18 2018 12:10 PM	More clean signage of places to park that are already available.
19	Jun 17 2018 08:49 PM	More parking
20	Jun 16 2018 12:49 PM	Improved signage for businesses
21	Jun 15 2018 04:28 PM	More parking



Question 20 Responses

Q 20: WHAT ARE THE GREATEST CHALLENGES YOU EXPERIENCE OPERATING A BUSINESS ON MAIN STREET?

Respondent	Response Date	Responses
1	Jun 30 2018 04:48 PM	Exiting the parking lot to north main
2	Jun 30 2018 04:21 PM	Parking
3	Jun 30 2018 02:57 PM	Lack of parking
4	Jun 25 2018 11:13 AM	Parking
5	Jun 22 2018 08:13 AM	Getting traffic in and out of the business.
6	Jun 21 2018 03:29 PM	Tree's need to be shaped or trimmed to allow signage to show up
7	Jun 19 2018 07:42 PM	Fast, loud, and pollutant traffic. Current trend of souped up diesel pickups intimidating pedestrians and store fronts. Maybe diminished with pedestrian friendly provisions as mentioned above.
8	Jun 19 2018 06:54 AM	Customer Parking
9	Jun 18 2018 10:56 PM	The City's seeming thirst for larger tax base by expanding everywhere but Main Street without supporting those who helped build the base through our efforts and taxes. Without current, longtime businesses on Main Street, there would be no base for the City to expand from.
10	Jun 18 2018 07:29 PM	Parking and residents on the east side of town who do not know we are here.
11	Jun 18 2018 06:04 PM	Getting people into my store
12	Jun 18 2018 04:43 PM	city unwilling to support businesses by enforcing existing ordinances
13	Jun 18 2018 01:41 PM	parking or lack there of
14	Jun 18 2018 01:40 PM	Parking. Speed and noise of traffic. Not pedestrian friendly for crossing the road. Getting people to actually stop and shop.
15	Jun 18 2018 01:26 PM	No parking for my customers, they complain about the lack of parking daily.
16	Jun 18 2018 01:00 PM	Other business' customers parking in my spaces.
17	Jun 18 2018 12:24 PM	Lack of Parking
18	Jun 18 2018 12:10 PM	Visibility from the street.
19	Jun 17 2018 08:49 PM	Traffic flow caused by lack of parking
20	Jun 16 2018 12:49 PM	Many people who come in say something like - I drive by here everyday and I didn't even know you were here. We put out an A-frame, we offset our building sign to account for the tree in front - but we just don't have the visibility we need to be successful
21	Jun 15 2018 04:28 PM	Parking and foot traffic